

# Analysis and Guidelines

## **Town of Lewisboro Special Character Districts**

Old Goldens Bridge  
Cross River  
Mead Street  
South Salem  
Elmwood Road

Prepared by  
The Town of Lewisboro  
Architecture and Community Appearance Review Council  
(ACARC)

# ANALYSIS AND GUIDELINES

## TOWN OF LEWISBORO SPECIAL CHARACTER DISTRICTS

Prepared by the  
Town of Lewisboro  
Architecture and Community Appearance Review Council (ACARC)

Council Members:     Gail C. Daigle  
                         Edward Delaney  
                         Geoffrey Egginton  
                         Douglas J. Gardner (1987-1988)  
                         Michael Havraniak  
                         Susan S. Henry  
                         Susan A. Landolt (1987-1989)  
                         Richard M. Pickett  
                         Thomas E. Lurcott, Chairman

Special Thanks To:   Edward Burroughs  
                         Theodore Chase  
                         Kathleen Cory  
                         Linda Green  
                         Colleen Brown

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### Appendix/Supporting Documentation

- A.    Town of Lewisboro Master Plan, 1985;  
        excerpts regarding SC Districts, pages 120-133
- B.    Enabling Legislation for ACARC;  
        Local Law #3-1987
- C.    ACARC General Guidelines
- D.    ACARC Procedures -

ANALYSIS AND GUIDELINES

TOWN OF LEWISBORO  
SPECIAL CHARACTER DISTRICTS

Old Goldens Bridge  
Cross River  
Mead Street  
South Salem  
Elmwood Road

## SPECIAL CHARACTER DISTRICT

### Analysis/Guidelines

#### INTRODUCTION

The Town of Lewisboro's 1985 Master Plan established five Special Character Districts, which are considered to merit particular attention due to the quality of their architectural and landscape settings. The Districts include areas delineated at Old Goldens Bridge, Cross River, Mead Street, South Salem and Elmwood Road. It is the intent of the Master Plan to develop ways to protect and preserve these Districts, thereby promoting the general welfare of the larger community.

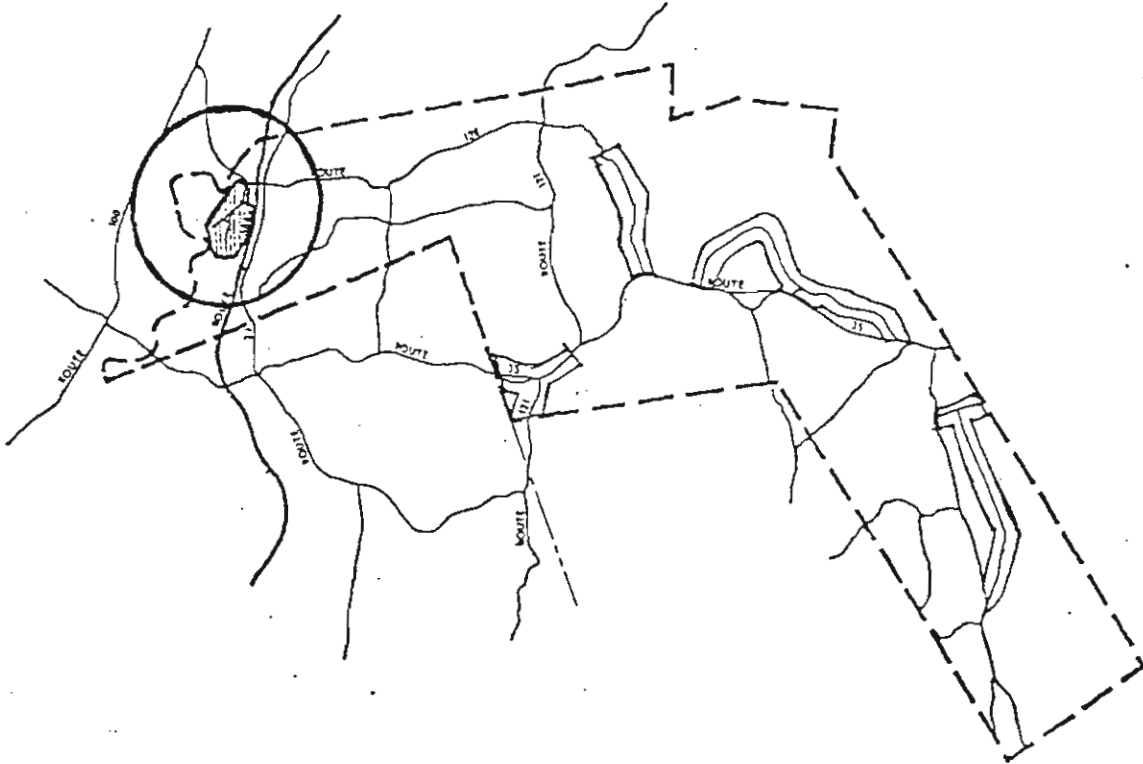
The Architecture and Community Appearance Review Council (ACARC) was established in 1987 to assist the Town in making the aesthetic judgements that affect the appearance and character of Lewisboro's built and natural environment. The development of review criteria for the Special Character Districts is required by the legislation that established those Districts, and has been a priority task for ACARC.

The purpose of this report is to establish review criteria and guidelines for use within Lewisboro's five Special Character Districts, where, because of the special quality of the architectural and landscape environment, a wider range of activities that affect community appearance is subject to Town review. The intent of the review criteria is to establish a measure by which the Special Character of these Districts can be defined, and a means by which that character can be protected and preserved.

ACARC has found each Special Character District to be unique and to be fully deserving of the special consideration implicit in the legislation that established them. The Analysis and Guidelines for each District follow. The Analysis attempts to separate out and define the physical characteristics that, in concert, make a special environment. The Guidelines suggest ways to protect those physical environments. The characteristics identified, such as special places or other special features, are not intended as an all-encompassing list, but as a series of positive examples to inform and guide the review process.

It is hoped that the Analysis and Guidelines will be a positive tool for the protection, preservation and enhancement of Lewisboro's special built and natural environments.

OLD GOLDENS BRIDGE  
SPECIAL CHARACTER DISTRICT



Historical Background

As early as 1750, maps and surveys show a Croton River crossing known as "Coldings," or Goldens Bridge. As a road and river intersection, it became a depot, then a community. Farm produce from Salem and Connecticut, along with goods from upper Croton River, were shipped via the Hudson River to New York City.

There were troop movements in the area during the Revolution. George Washington may have considered it as a crossing for troops headed for New York from a headquarters nearby.

## OLD GOLDENS BRIDGE

Goldens Bridge was part of the Van Cortlandt Manor until after the Revolutionary War. When New York became a state, manor lands were split among the neighboring towns. Goldens Bridge became a hamlet in the Town of Salem.

The early 19th century brought prosperity and growth for the farm community. In 1847, the New York and Harlem Railroad was extended to Goldens Bridge, which became a depot for produce from the local farms. A business community began to develop at the intersection of (now) Route 22 and Route 138. A branch of the railroad was extended to Lake Mahopac to transport summer residents there.

In 1901, the most drastic change took place. The Croton River Valley was flooded to form a reservoir of drinking water for New York City. Dozens of buildings were moved to new sites. A more modern business district grew to serve the increasing number of residents and commuters. This, too, was demolished when Interstate 684 was finished in the 1960s.

Original buildings still existing in the Hamlet of old Goldens Bridge include St. Michael's Chapel, the former Elementary School, now the property of the Town of Lewisboro, and the New York Store, now a residence. There is also a three-story building that was once a hotel. It had been moved from the site of what may have been Washington's headquarters near the Croton River. The road bed of the Mahopac rail spur is still visible.

Old Goldens Bridge has maintained its identity throughout the upheavals that mark its history, and its character survives.

## OLD GOLDENS BRIDGE SPECIAL CHARACTER DISTRICT

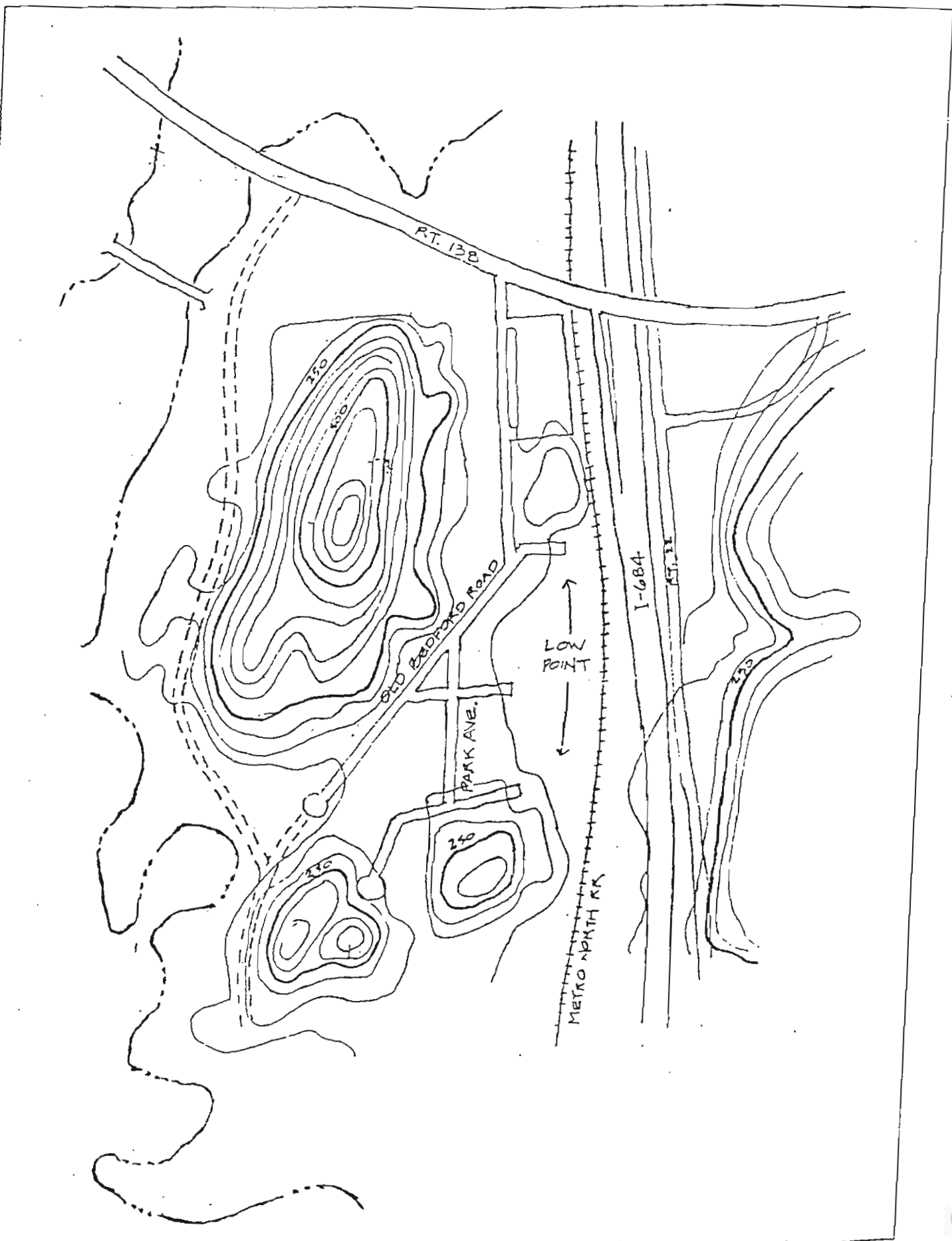
### ANALYSIS OVERVIEW

Old Goldens Bridge is a Special Character District consisting of a small, tightly-knit village enclosed in a picturesque hamlet setting. However, two distinct uses within the district, one residential and the other general business, are in conflict due to shared traffic access. The intent of these guidelines is to mitigate the conflicts of these adjacent uses and to preserve the special qualities of the hamlet.

### GUIDELINES SUMMARY

1. Preserve the existing topography.
2. Preserve and maintain existing landscape features. Maintain and enhance landscape buffers between dissimilar uses.
3. Preserve existing special places and uses including the village "green", the old Schoolhouse/Goldens Bridge Community House/NWCA, St. Michael's Chapel, the surrounding reservoir trails and the old railroad bridge.
4. Alterations and construction of new houses within the old village proper will be subject to design criteria in order to relate to the existing traditional houses in the district.
5. Undesirable views from within and from outside the district will be screened utilizing landscape buffers.
6. An alternate access route for truck traffic is highly recommended to reduce conflicts between residential and general business uses.





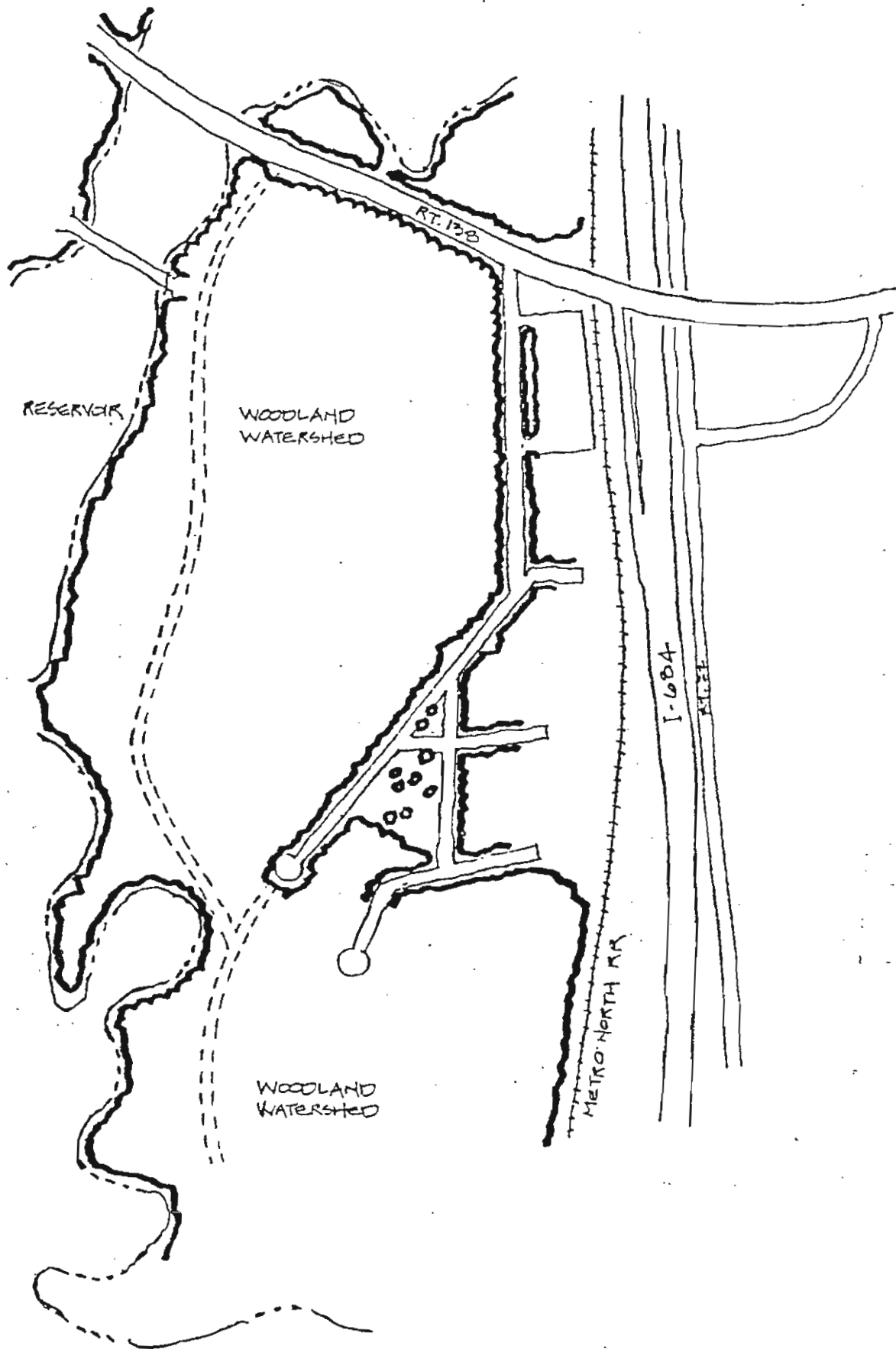
## OLD GOLDENS BRIDGE

### 1. TOPOGRAPHY

The Old Goldens Bridge Special Character District is surrounded on three sides by reservoir and sheltered all around by wooded hillsides that give the hamlet its characteristic intimacy and enclosure.

The village itself, centered on the green bounded by Old Bedford Road and Park Avenue, is a fairly flat shelf defined to the west and south by rising hillsides. The grade slopes down slightly from the village green to the adjacent general business area, where it reaches its low point.

Guidelines - A. The natural topography strongly defines the boundaries of the village as well as the precincts of the residential and business uses. The existing topography should not be altered.



## OLD GOLDENS BRIDGE

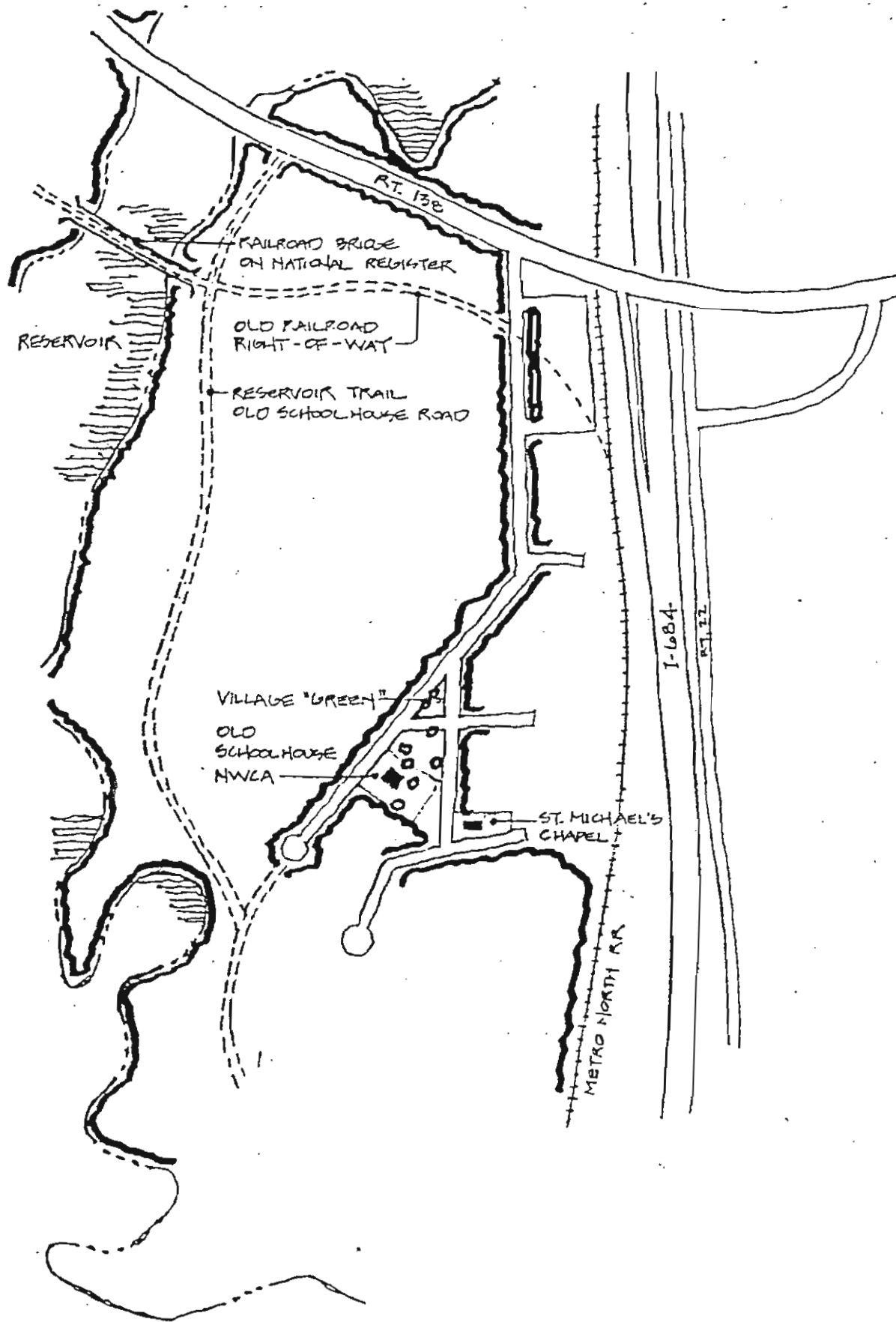
### 2. LANDSCAPE FEATURES

Densely wooded watershed lands surround the district to the north, west and south reinforcing the sense of enclosure created by the hillsides.

The tree canopy opens up at the center of the village where informally placed large, mature trees shelter the residential portion of the district. Existing trees and other landscape elements provide some visual screening of the adjacent general business area and Interstate 684, but the buffer is thin, random and unprotected. The maintenance of a strong landscape buffer between the residential and general business areas of the hamlet is critical to the well-being of the residential area.

Starting at the village "green", the landscaping is informal and naturalistic. The roadway edge is informal and fences, while in a variety of forms, are most often low and transparent.

- Guidelines - A. The wooded watershed and the existing mature trees within the village proper are critical to the special character of the district and are to be preserved and protected.
- B. A zone running between the residential and general business precincts is identified as a critical landscape buffer area. A vigorous evergreen landscape buffer should be developed and maintained to supplement existing deciduous plantings.
- C. Residential landscape treatments should be informal and green. Fencing should mostly be low and transparent.



## OLD GOLDENS BRIDGE

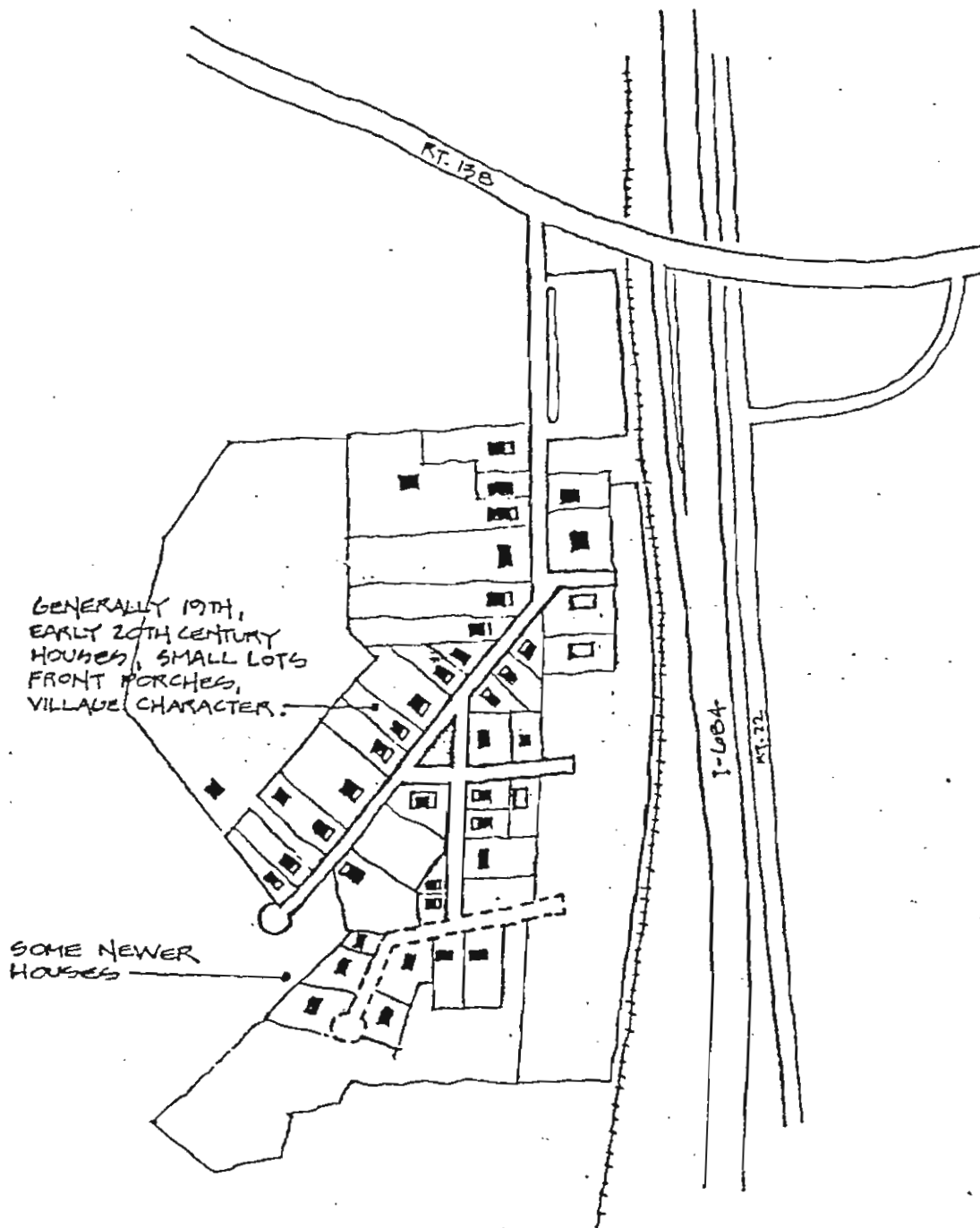
### 3. SPECIAL PLACES

Special places in the old village occur at both the village center and in the surrounding woodlands.

In the village center, the triangular "green" bordered by Old Bedford Road, Park Avenue and Meadow Street is the symbolic middle of the village and a pleasant, green open space. The nearby St. Michael's Chapel and the Goldens Bridge Community House (former schoolhouse) are assets to both the village and the larger community, as well as attractive older buildings.

The watershed woodlands that surround the village feature pleasant trails as well as the old railroad bridge on the former Lincolndale Spur. The bridge is on the National Register of Historic Places.

Guidelines - A. The special places cited above are amenities to both Old Goldens Bridge and to the larger community and are to be preserved and protected.



## OLD GOLDENS BRIDGE

### 4. HOUSES

Houses in the old village proper, that is on Old Bedford Road and Park Avenue, are remarkably consistent in their characteristics. Generally late 19th to early 20th century, modest rural Victorian homes, their density and resulting sense of community is unique to Lewisboro. The lots are generally narrow and deep. Almost every such house faces its narrow entrance side, usually with a front porch, to the street. The intimacy and closeness of the hamlet is further reinforced by the closeness of the houses to each other and to the street, all being surrounded by hills and woodland.

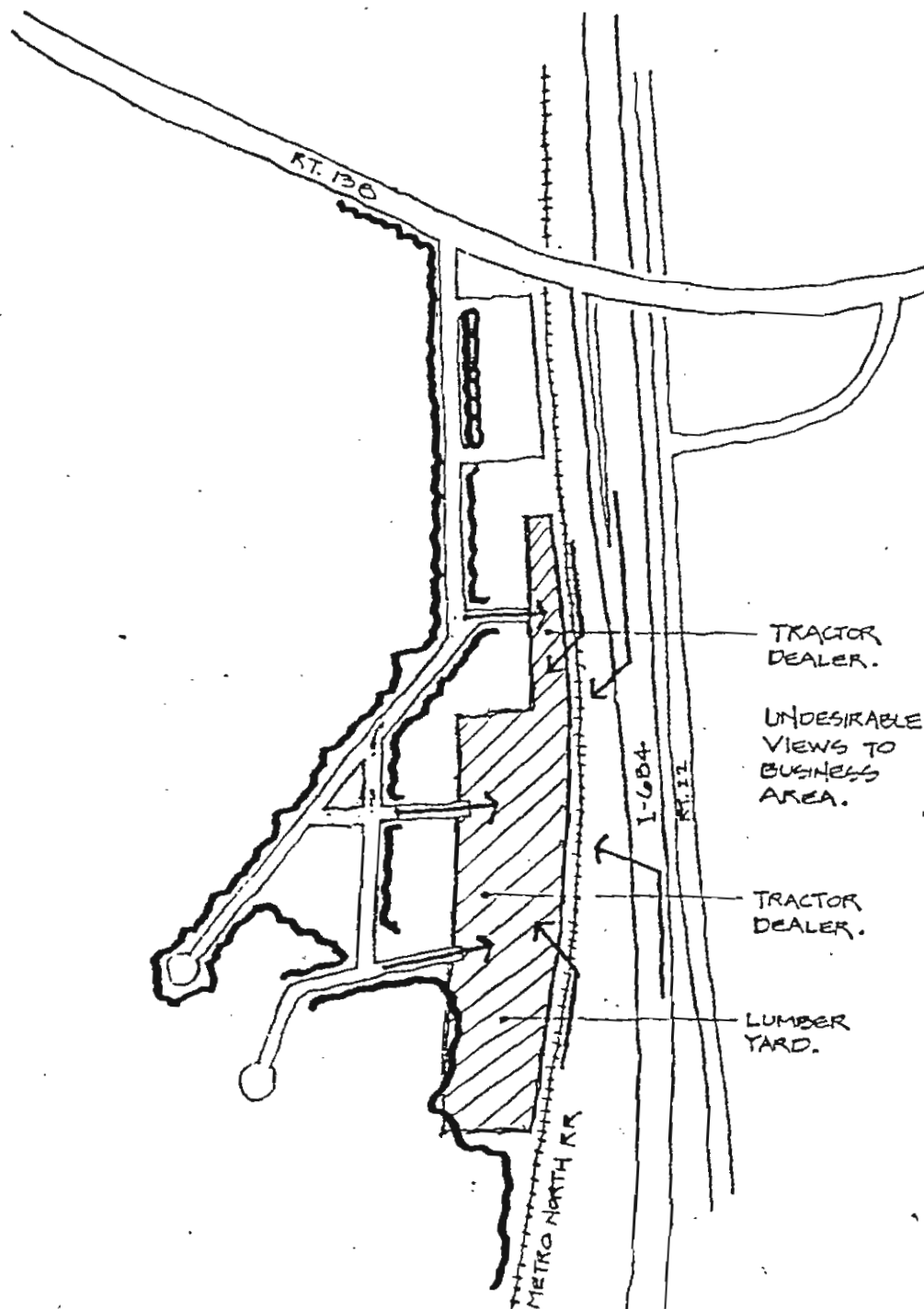
Some newer houses have been built on Mandia Lane, which is a private road and separate from the village center.

Several residential structures are abandoned or in only marginal use. These stand on less desirable lots immediately adjacent to general business uses. The preservation of the residential character of the properties is strongly encouraged.

Guidelines - Houses built new or altered in the old village proper, on or visible from Old Bedford Road or Park Avenue, will be subject to design criteria. The intent of these criteria is to insure that the consistency of the scale and character of the existing residential buildings is maintained. The design criteria are as follows:

- A. New houses in the village proper will be of frame construction and sided primarily in wood. They will be two to two and one-half stories high and will have roofs with a minimum pitch of nine in twelve.
- B. New houses will have front porches and entrances facing the street, and they will be set back from the street no more than their immediate neighbors.
- C. Windows and trim of new houses will be of proportion and scale similar to those of the existing old village houses.
- D. Alterations to existing old village houses will be consistent with and maintain the architectural integrity and detail of the original house.





## OLD GOLDENS BRIDGE

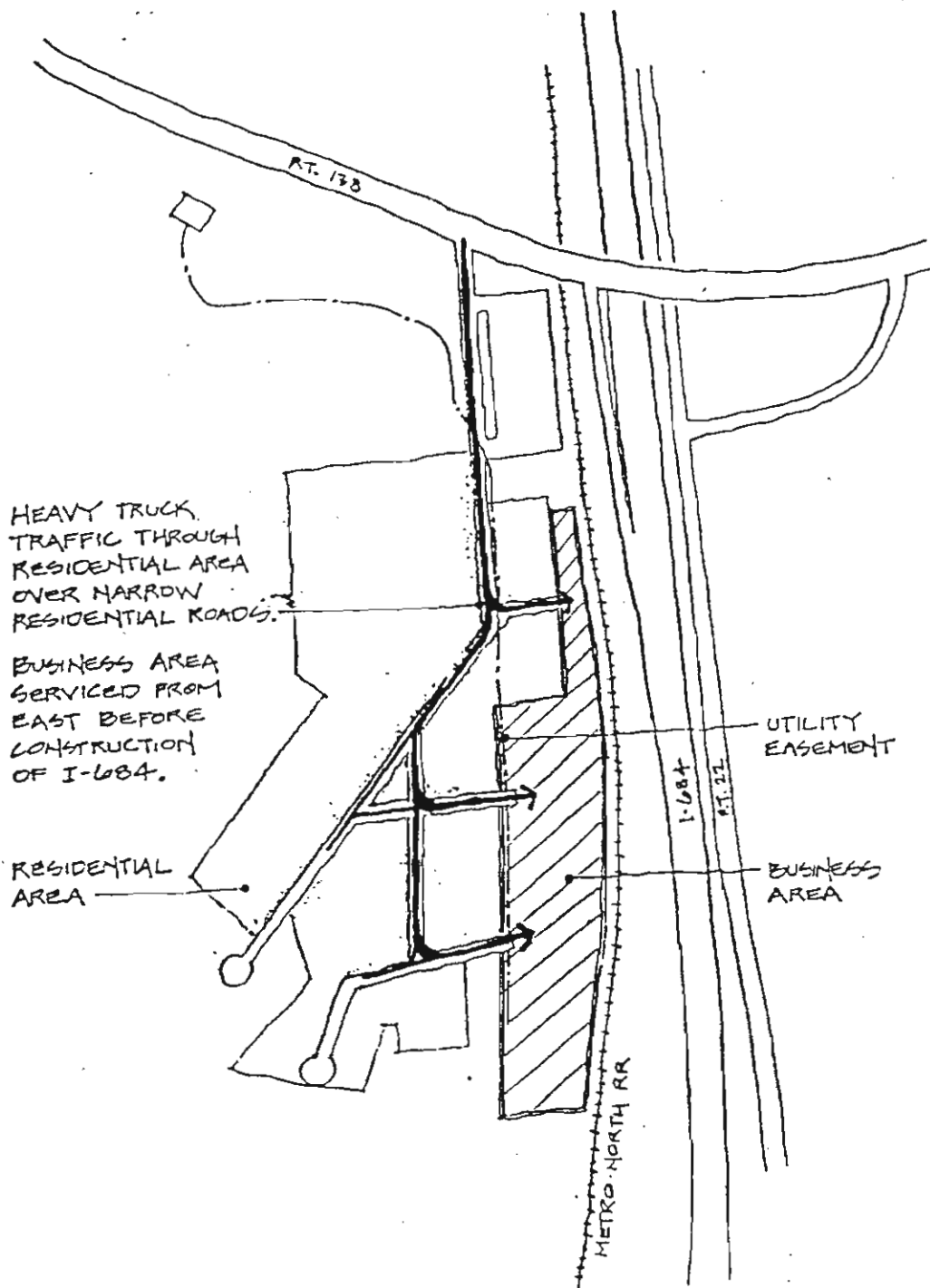
### 5. VIEWS

As previously described, Old Goldens Bridge is a small hamlet village, sheltered by surrounding hills and woodlands, so that views and vistas from within the district do not occur. However, there are undesirable views that relate to the general business area.

From within the district, undesirable views to general business occur as one walks down North Street, Meadow Street and Mandia Lane. Views of the business area down the street corridor are inevitable, but are mitigated with street trees. Mid-block views to the general business area also occur where the landscape buffer is thin, as mentioned under Landscape Features.

Views to the hamlet's only public face--that facing the railroad, Interstate 684 and Route 22--feature stored lumber and tractors, offering no clue to the charming residential hamlet beyond.

- Guidelines -
- A. A landscape buffer between the residential and business areas of the district will be maintained to screen the undesirable views of the business area, Interstate 684, etc, from the residential portions of the district.
  - B. Improved landscape treatments of the business area are recommended to upgrade the appearance of the public face of the old village. Plantings, especially along the railroad edge of the business area, would go far towards accomplishing this goal.



## OLD GOLDENS BRIDGE

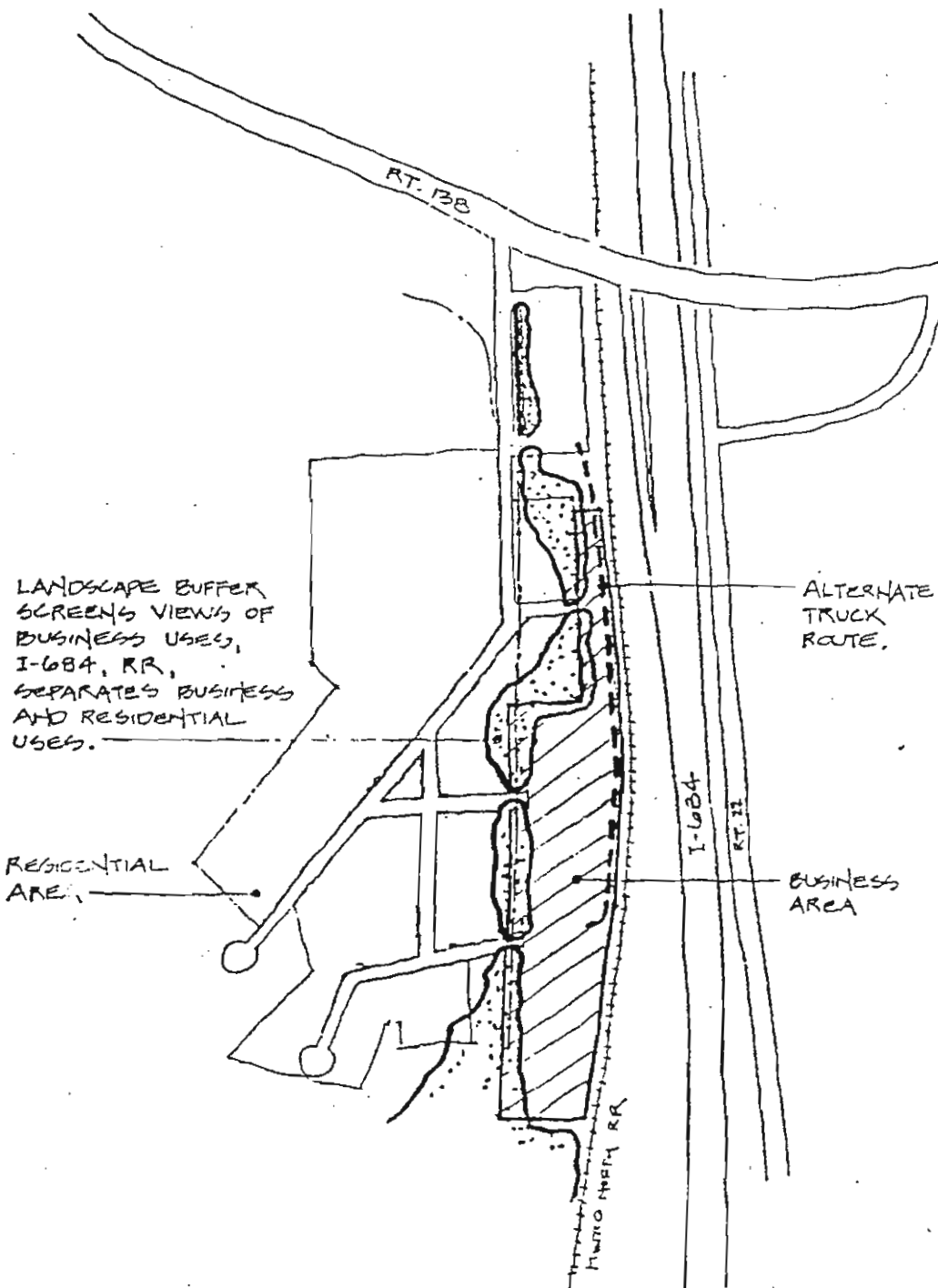
### 6. TRAFFIC/USE

Special attention is required to address the problem of separating the residential and business areas.

The general business area lies between the residential area and the railroad/I-684 corridor. Access to the general business area was formerly from the east, directly from Route 22. When I-684 was built, that access was cut off and traffic was forced to enter via Old Bedford Road only. In recent years the businesses have expanded and have grown more dependent on truck than on rail delivery; consequently, the trucks have become increasingly larger.

Today, fifty-five foot tractor trailers attempt to maneuver and turn on the narrow residential streets, causing visible damage to roads, yards, and trees and significant danger to residents.

It is critical to the viability of Old Golden's Bridge that action be taken to mitigate the growing impact of truck traffic in the residential area.

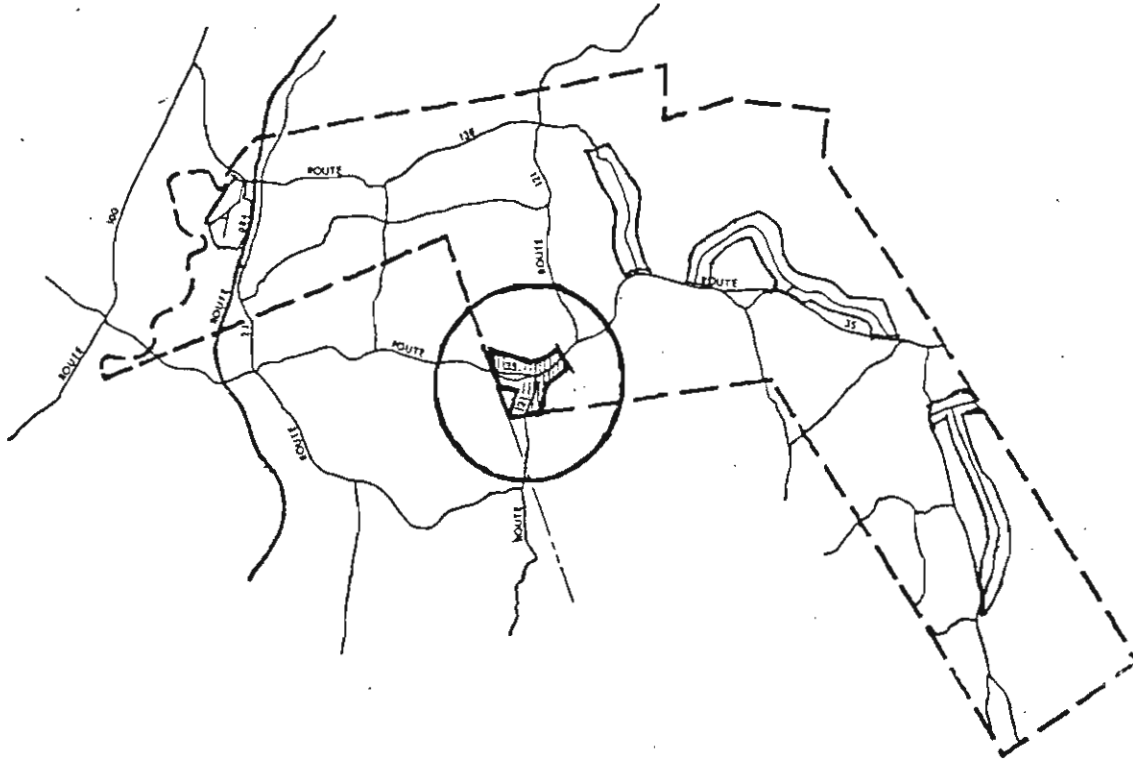


7. TRAFFIC/LANDSCAPE GUIDELINES

Guidelines- A clear separation of residential and business use and access to each is essential. Separation of use is accomplished by zoning boundaries, the natural grade change and landscape buffer.

- A. Separate, landscaped access road - The separation of access is dependent upon the development of an alternate road for truck traffic to the business area. There appears to be a route parallel to the railroad track to accomplish this and it is strongly recommended. A new access road with appropriate landscape treatments would put a positive, new face on the old village's public edge and allow the business and residential uses to prosper side by side.
- B. Landscape Buffer - As discussed in Item 2B, a landscape buffer is critical to the visual separation of the residential and business uses.

CROSS RIVER  
SPECIAL CHARACTER DISTRICT



Historical Background

Cross River was founded sometime after 1723, built at a point where two rivers meet, the Indian River and Cross River. The Indians called it "Pepeneghek" which meant "boundary place", or "place marked out".

The land was once part of Lot Nine, a division of Cortlandt Manor. A number of houses along Route 121 and Route 35 date back to manor days. Nearly all the homes of the period, up until the early 1800's, had central chimneys. Most were built on stone foundations and sided with wooden clapboards.

## CROSS RIVER

By the mid-1800s, Cross River was becoming a village of some note and the end of the century brought a rise in the standard of living.

In 1905 the dam was built, creating the reservoir and the configuration of Cross River we recognize today. The Cyrus Russell House, once a schoolhouse, and the old cemetery are further reminders of Cross River's history.

Because of the significant historical value, it is important to preserve Cross River.



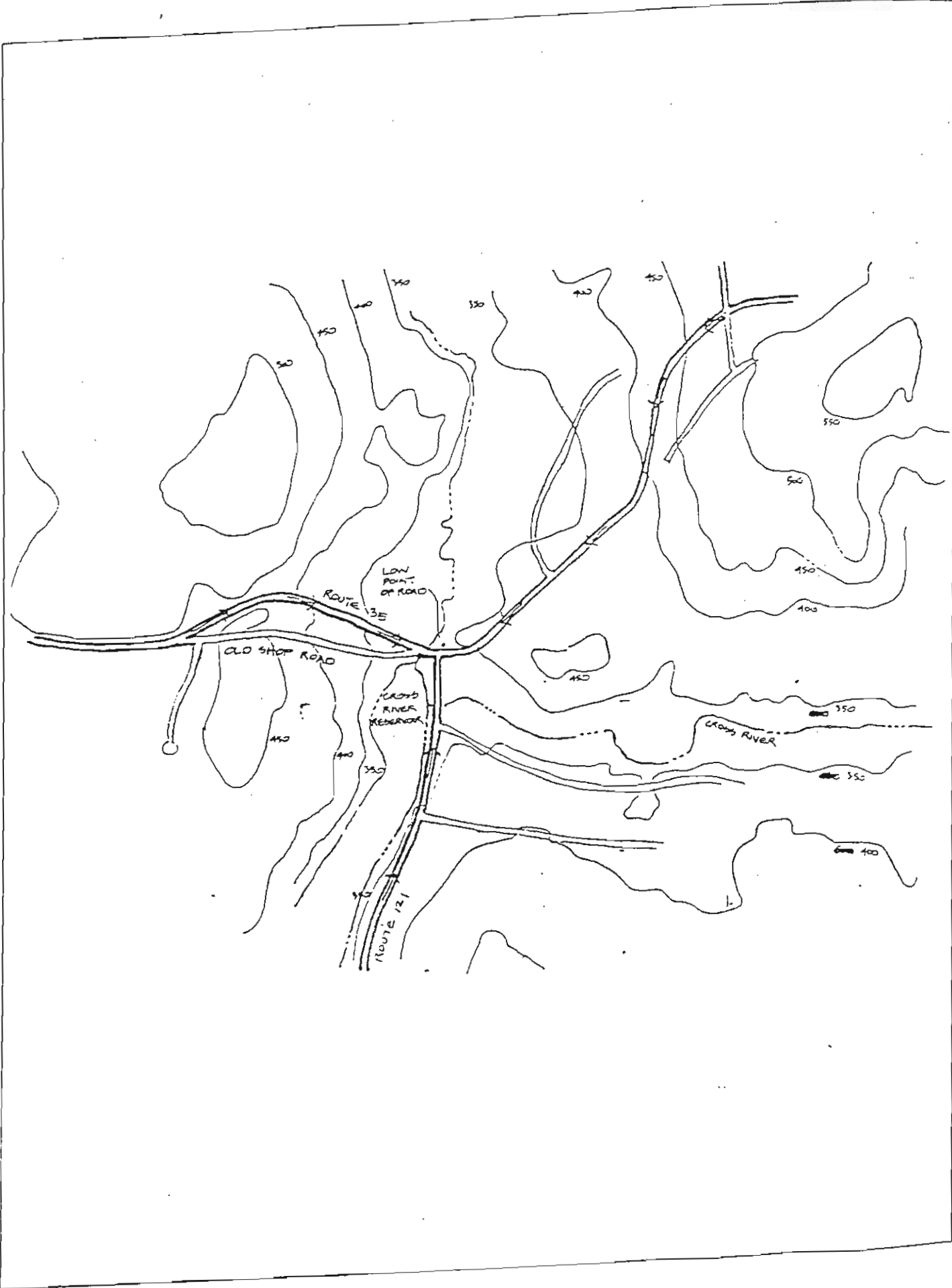
## CROSS RIVER SPECIAL CHARACTER DISTRICT

### ANALYSIS GUIDELINES

Cross River is a Special Character District that falls in an attractive natural crossroads, featuring a mix of uses and unfortunately, heavy, rather high-speed traffic. Due to traffic volume, there is a lack of pedestrian activity, and there is no overall sense of place. Instead, there are fragile pockets of special character. The intent is to protect these pockets from surrounding traffic and development pressures.

### GUIDELINES SUMMARY

1. Preserve the existing topography.
2. Preserve the major existing landscape features as well as elements defining roadway edges such as tree lines, stone walls and grade changes. Maintain landscape buffers to nearby dissimilar uses.
3. Preserve existing views and vistas.
4. Uses within the district can be mixed but should work within a residential building type.
5. Alterations and construction of new houses should relate closely to the scale, siting, and materials of existing Cross River buildings.
6. Preserve and maintain the existing special places.



## CROSS RIVER

### 1. TOPOGRAPHY

The Cross River Special Character District is formed by the confluence of two streams, which meet at the Cross River Reservoir, and by the hills that rise above the streams. The specifics of the topography also define the corridors of two rather old and well-traveled roads, Routes 35 and 121.

The lowest elevation of the district occurs at the intersection of Route 35 and Route 121. Route 35 then rises quite sharply to the west, and more gradually to the east. Route 121 rises gently, as it heads south past the Cross River Reservoir toward Bedford.

Guidelines: A. The existing natural topography of Cross River, the hills that form the drainage and roadway corridors, is an essential part of the character of the district and should be preserved.

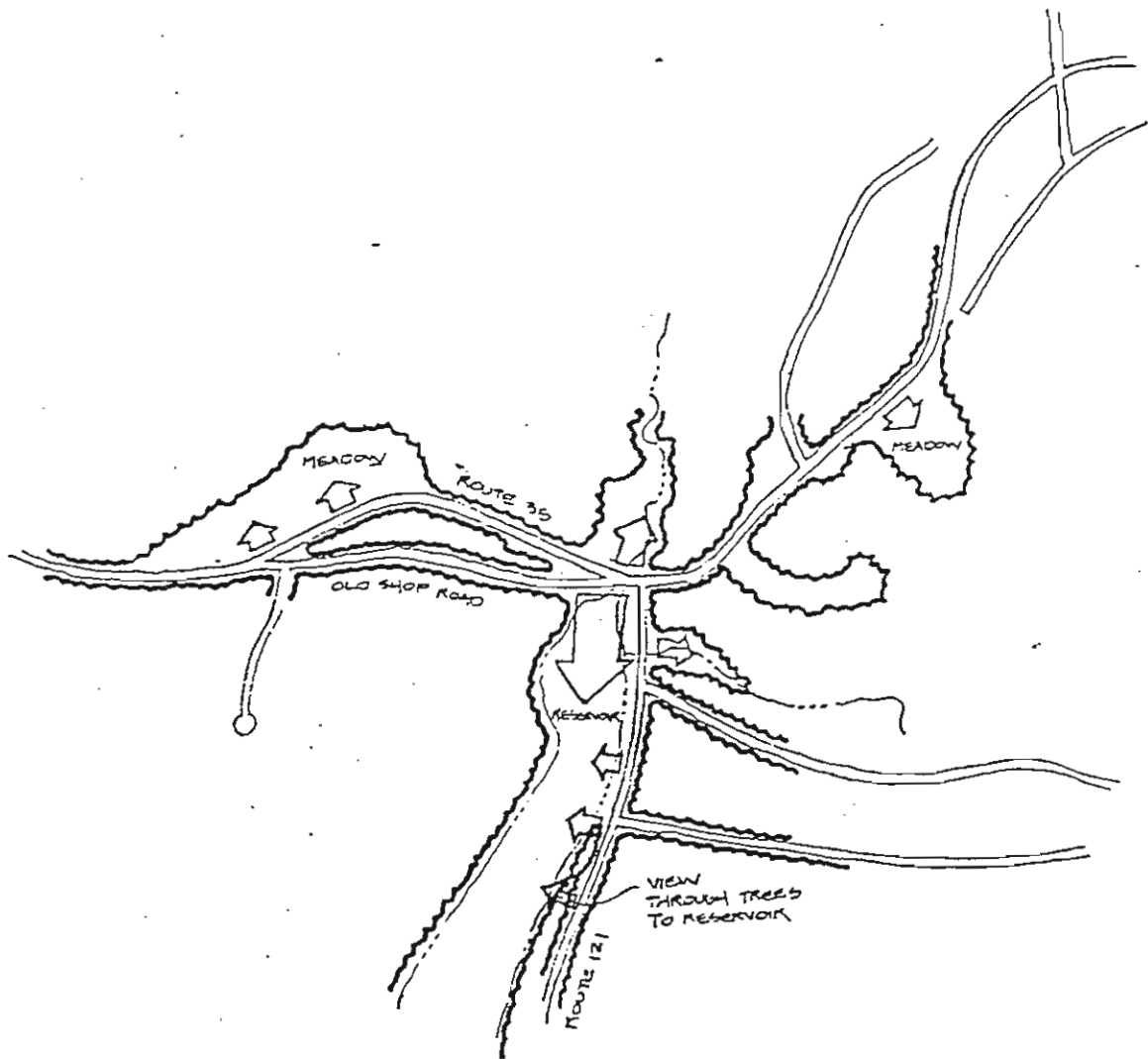
## CROSS RIVER

### 2. LANDSCAPE FEATURES

The most dominant landscape features are a function of site topography. Of these, the Cross River Reservoir and its two feeding streams and adjoining wetland areas are most attractive. The adjacent hills are generally wooded, with open areas carved out by the roadway corridor, or by other development.

The roadway corridors of Routes 35 and 121 are defined variously by tree lines, grade changes, stone walls and along Route 121, by the reservoir. As Route 35 rises to the west and east, the corridors open up. To the west, a grassy meadow opens at the Four Winds entrance. To the east on Route 35, open areas occur along several residential properties and a landscape buffer, well placed but small, screens the Meadows multi-family development.

- Guidelines:
- A. The Cross River streams, wetlands and reservoir are important and attractive natural features and should, of course, be preserved.
  - B. The tree canopy and the roadway edge defining elements, (tree lines, stone walls, grade changes) should be preserved.
  - C. Landscape buffers to screen dissimilar uses, such as multi-family housing, should be developed and protected.



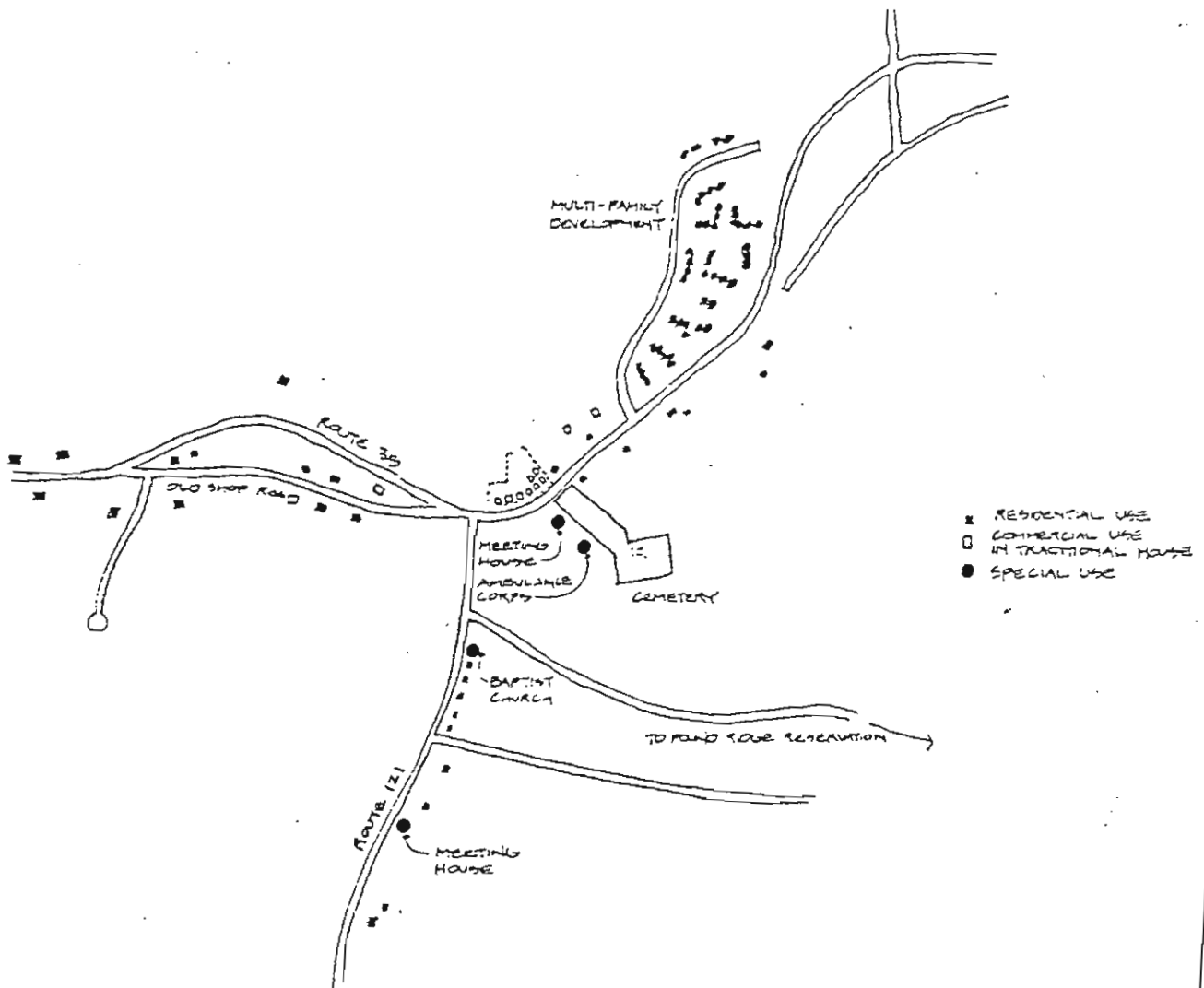
## CROSS RIVER

### 3. VIEWS/VISTAS

The Cross River roadway corridors are constantly changing grade and curving, so that views tend to occur perpendicular to them. As one traverses Route 35 from east to west through the district, views open over residential properties to the south, to the reservoir to the south, to the stream to the north, and finally over the meadow at the Four Winds entrance.

As one proceeds south on Route 121, views up Cross River and down to the reservoir punctuate the drive. As the road rises, the views to the reservoir begin to be through the canopy of trees, until gradually they are obscured by forest.

Guidelines: A. The above mentioned views and vistas should be preserved and maintained.



## CROSS RIVER

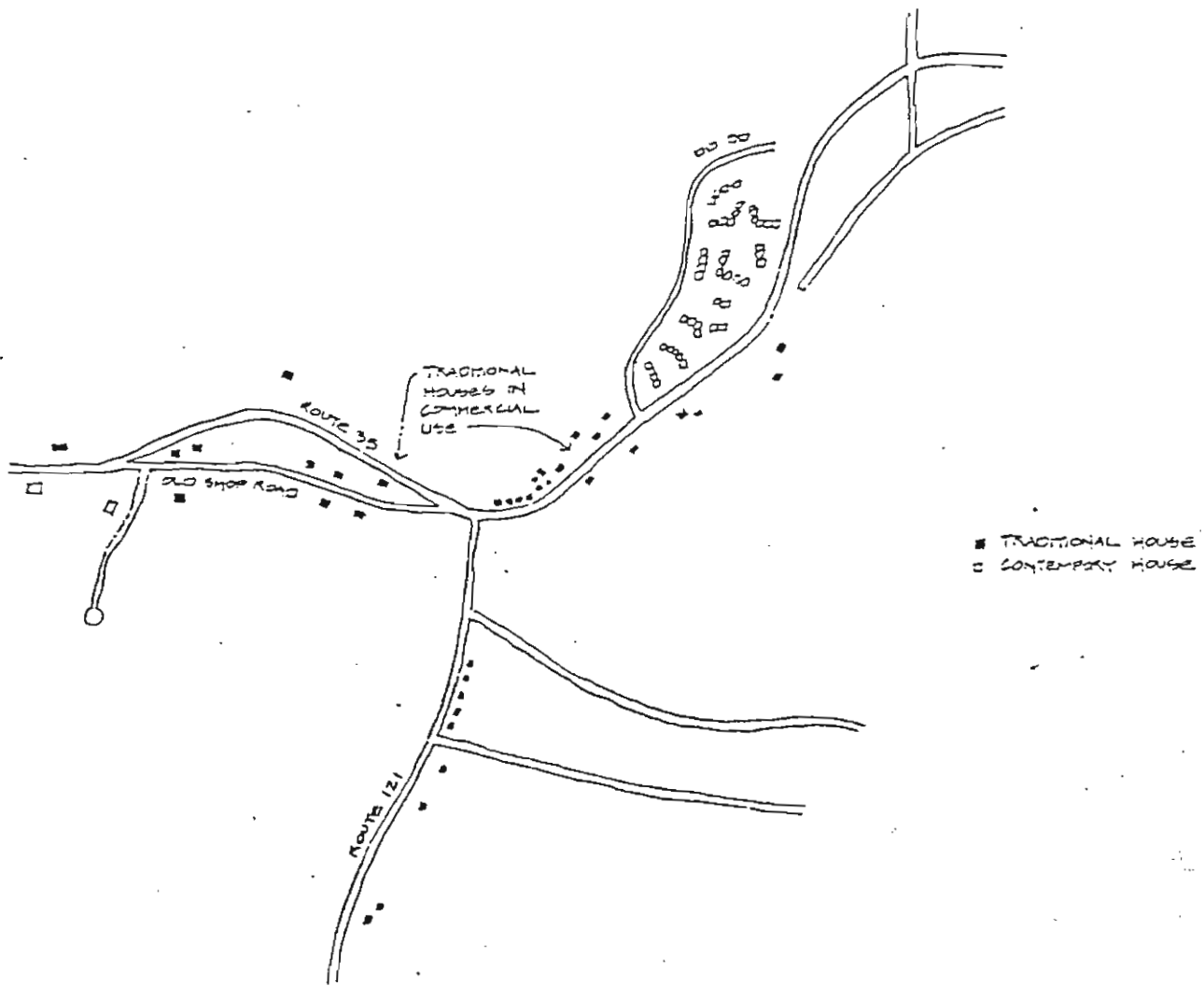
### 4. USE/TRAFFIC

Cross River is a mixed use district including single family residential, light commercial and professional uses. Secondary uses include assembly functions such as a church and the Cyrus Russell meeting house, and the Lewisboro Volunteer Ambulance Corps building. The mix of uses is generally compatible with the overall residential character of the district since the uses all occur within older, formerly residential structures. Only the Meadows multi-family development, outside the district but abutting it, would be considered a non-compatible use.

Unfortunately, the mixture of uses and the crossroads location results in a heavy volume of traffic at rather high speeds. The traffic, along with the lack of pedestrian activity, prevents any overall sense of place from being perceived in Cross River.

- Guidelines
- A. Use in the Cross River Special Character District should be restricted to the kinds of uses that exist there now, those that can function in a building of residential scale and appearance.
  - B. While little can be done to reduce traffic volume, it is suggested that any future roadway improvements would be undertaken with a sensitivity to the special character of Cross River.
  - C. Finally, pedestrian activity would benefit the use and the sense of place within the district, but this requires the presence of pedestrians. Improved pedestrian walkways through the district to present and future residential areas should be considered.





## CROSS RIVER

### 5. HOUSES

The mix of uses in Cross River is accommodated almost entirely in structures of a 19th century residential style and scale. The character, appearance and siting of these structures is critical to the special character of Cross River.

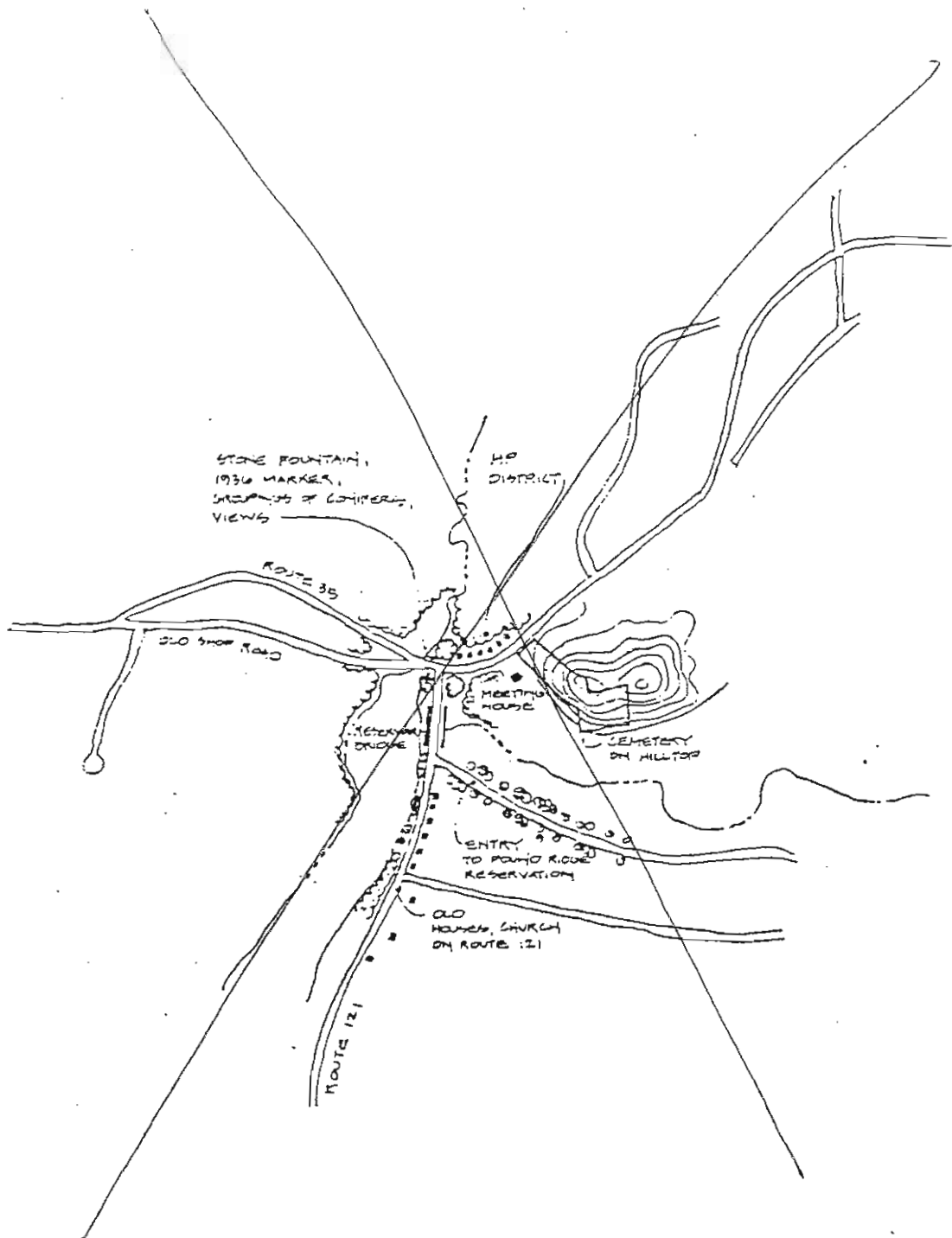
The houses and church along Route 121 are probably of the most historical and architectural interest and are cited below in Special Places. They are intimate in scale, close to the road and to each other, and date from the late 18th to the early 19th century.

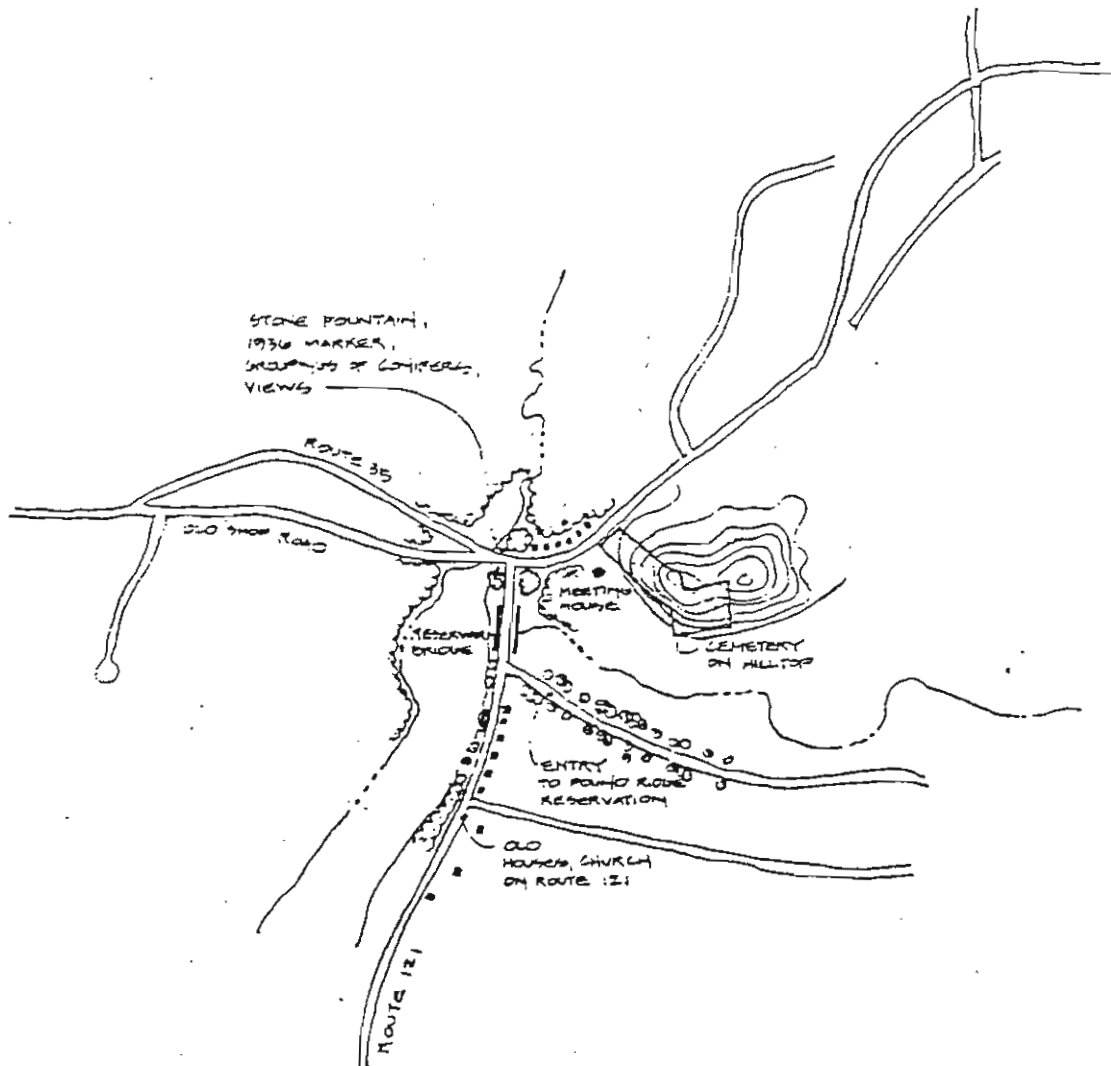
Route 121 meets Route 35 where a series of tightly spaced, formerly residential structures are now used for retail stores. These uses, which include Yellow Monkey Village, capitalize on the handsome 19th century character of the structures and benefit accordingly.

Houses on Route 35 to the east of the Hamlet Preservation District are a mixed lot in both appearance and use. However, they tend to be of 19th century vintage, on large, open lots and set back from the road.

Houses to the west of the Hamlet Preservation District occur primarily on Old Shop Road, where a heavy tree canopy and a strongly defined roadway edge have a larger impact than the individual houses.

- Guidelines
- A. Alterations to any of the 18th or 19th century houses in the district should be undertaken with care and in a manner consistent with their original style and residential use, even if their new use is non-residential.
  - B. New construction should be sympathetic to the residential scale, massing and materials of the existing houses, (frame construction, pitched roofs) and respect the lot line and street line setbacks of their immediate neighbors. No matter what the use, it is a residential appearance and character that should be the measuring stick of fit within the Cross River District.





## CROSS RIVER

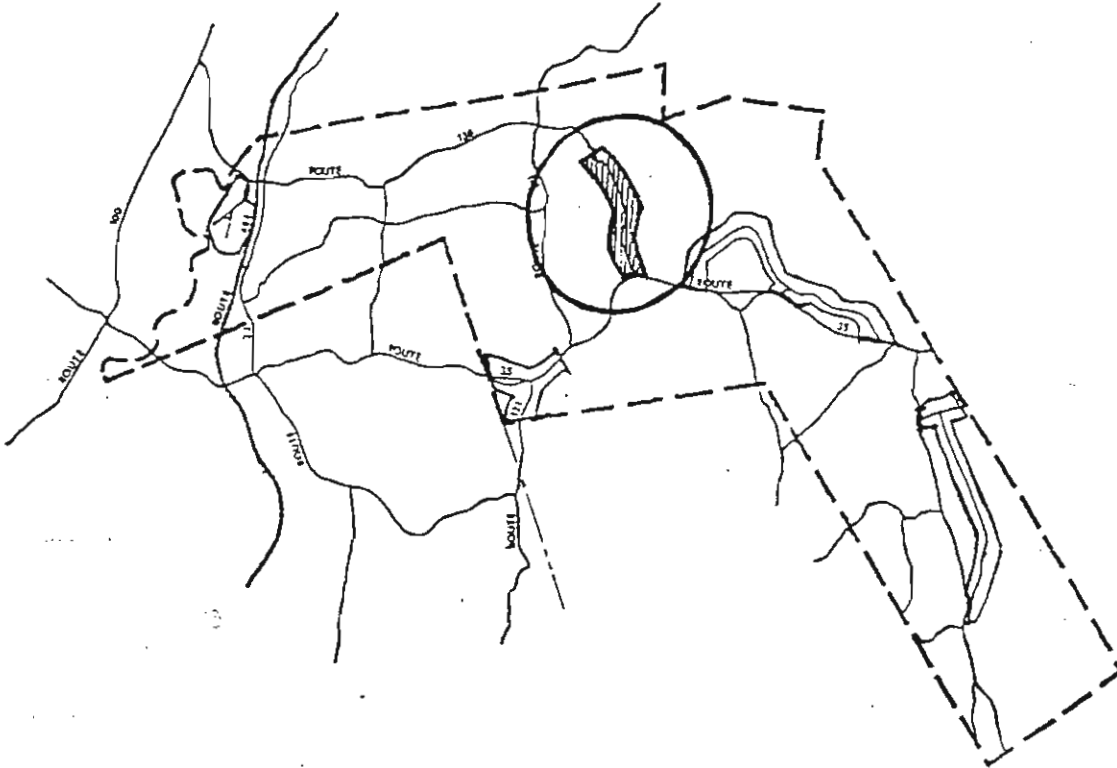
### 6. SPECIAL PLACES

Special places within the Cross River district can be described as pockets that occur along Route 121 and at and nearby its intersection with Route 35.

The original settlement in the district was along Route 121 and the surviving houses and the Baptist Church are of the most historical and architectural interest. As Route 121 continues toward the Route 35 intersection, the Pound Ridge Reservation entrance, and the closeness to the reservoir, the streams and the wetlands highlight the natural beauty of the district. Finally, at the Route 35 and 121 intersection, the Cyrus Russell House, (formerly a school house) and the cemetery climbing up the hill give Cross River a symbolic, if not actual, center.

Guidelines: A. The special places mentioned above are particularly fragile pockets of historic and natural landscape settings. Development and traffic pressures should be resisted in order to preserve and maintain them in their present condition.

MEAD STREET  
SPECIAL CHARACTER DISTRICT



Historical Background

The first Meads settled at the eastern edge of the Van Cortlandt Manor just before the Revolutionary War. Until the early twentieth century, Mead Street was a self-sufficient farming community.

The Mead Memorial Chapel was built in 1905 by Sarah Frances Studwell Mead in memory of her husband, George. Her children built their own homes along Mead Street where they planted maple and elm trees, which still line the roadway.

## MEAD STREET

Their relative, Robert Hoe, made a summer estate where the Waccabuc Country Club now stands. He was the benefactor who built the Post Office and the School House in the 1880's.

The Mead descendants, along with more recent settlers, are still aware that Mead Street is a very special heritage.

## MEAD STREET

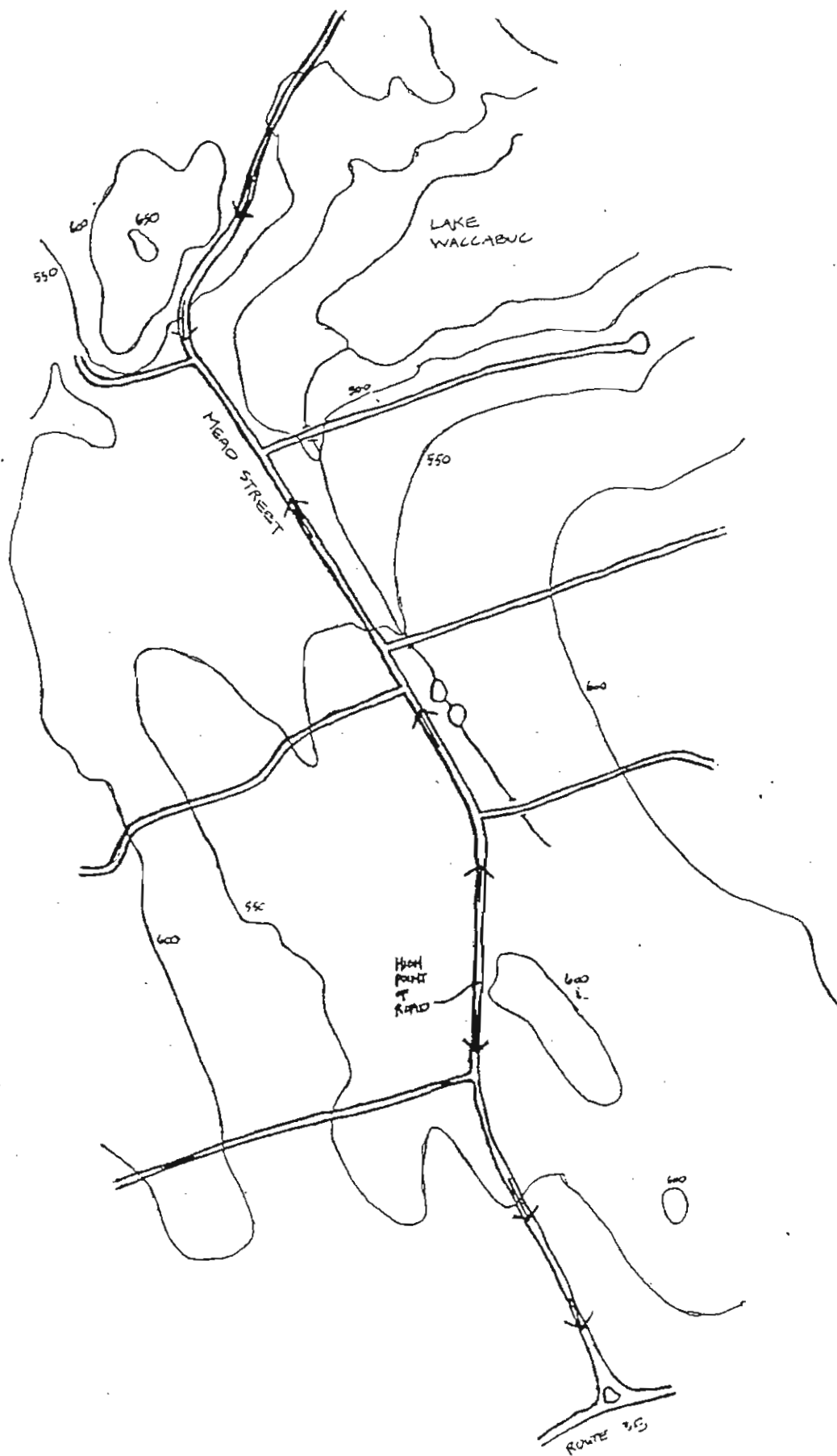
### ANALYSIS OVERVIEW

Mead Street is a Special Character District with an exclusively residential use, low traffic and a naturally beautiful rural landscape. Historic homes add a sense of permanence and continuity. The intent is to preserve the existing special character of Mead Street.

### GUIDELINES SUMMARY

1. Preserve the existing topography/landscape.
2. Preserve and maintain the existing roadway edge consisting of mature street trees, stone walls, fences and grade changes.
3. Preserve the existing views/vistas.
4. Preserve the existing special places and uses, including the Mead Cemetery, the Waccabuc Country house, the Post Office and Mead Memorial Chapel.
5. New construction sited prominently with respect to Mead Street will be subject to design criteria in order to relate to the existing traditional style homes in the district.





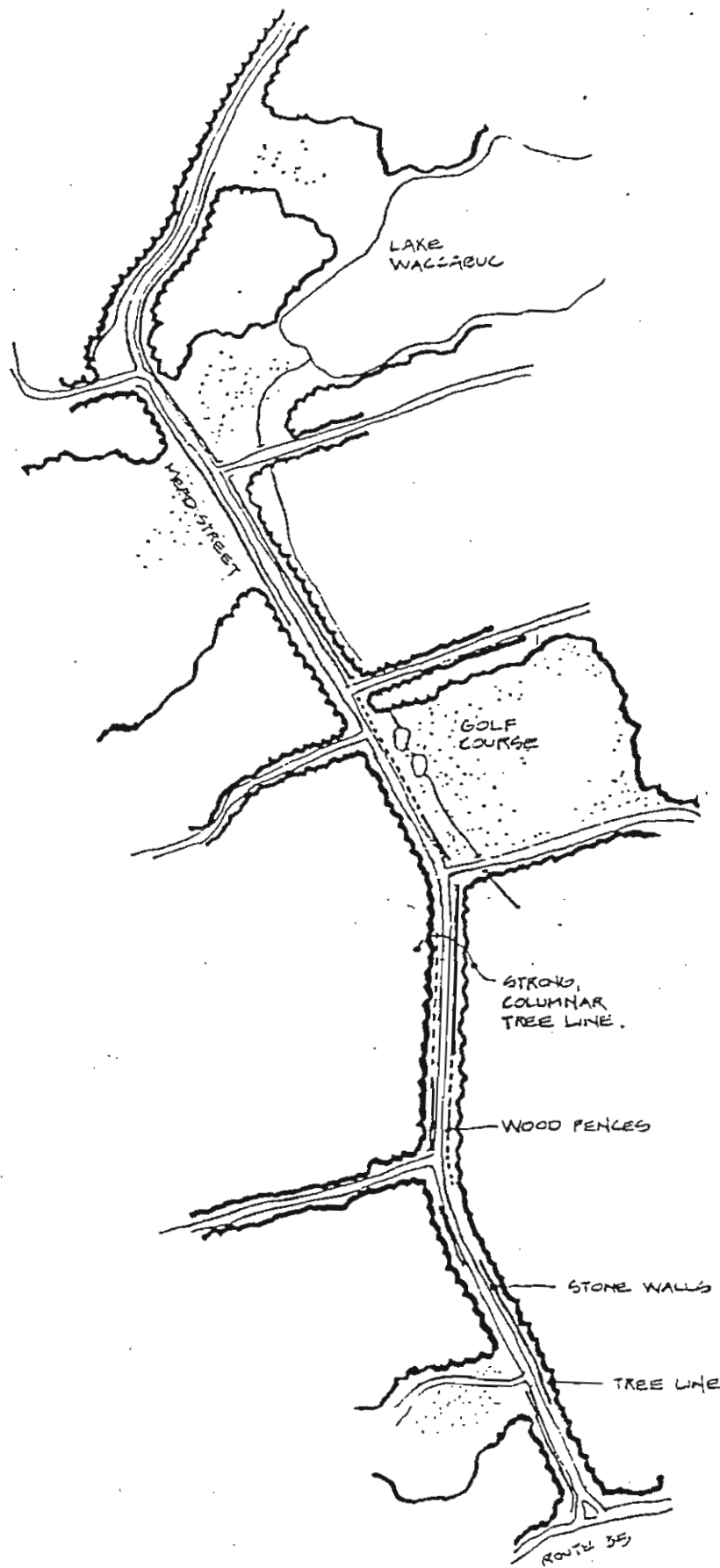
## MEAD STREET

### 1. TOPOGRAPHY/LANDSCAPE

The terrain in the immediate area of Mead Street consists of gently rolling woods and meadowland, once fertile though rocky farm land.

Mead Street rises gently as it runs north from Route 35 past a wooded ravine to the east. The slope continues upward past Schoolhouse Road, and reaches a crown several hundred yards beyond. Mead Street then begins a long, gradual down slope. Twin ponds at the Waccabuc Country Club golf course are the first visible signs of a stream that parallels the road to the east. Mead Street reaches its lowest point just past Tarry-a-Bit Road, where a meadow and along with it, the stream, slope down to Lake Waccabuc. Mead Street then winds sharply up past the Mead Memorial Chapel. A steep ridge rises above the road to the west, while grade slopes gradually down to Lake Waccabuc to the east, allowing an occasional glimpse of the water below.

- Guidelines - A. The essence of Mead Street topography and landscape is its natural, undisturbed character, which should be preserved. Existing grades and patterns of drainage should not be altered.
- B. Mature trees, whether individuals or groups, should be preserved and maintained.



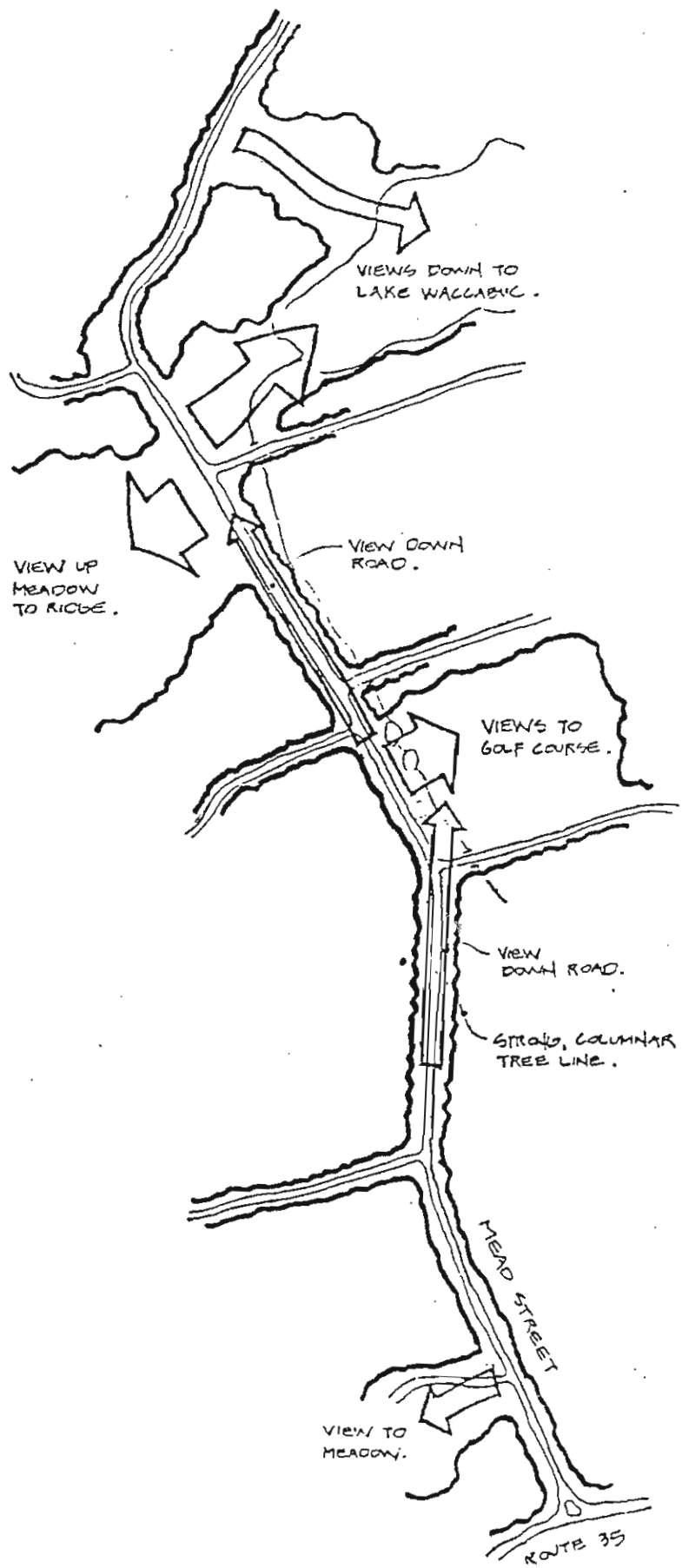
## MEAD STREET

### 2. ROADWAY DEFINITION/EDGES

For virtually its entire length, the Mead Street roadway is defined on both sides by a combination of stone walls, wooden fences, grade changes and mature street trees. The interaction of these elements gives Mead Street its strong rural character: a sharply defined carriageway that is shared by vehicles and pedestrians, without curbs or sidewalks. A clear delineation between the public roadway and the private domain beyond is achieved.

The roadway edge is strong although generally transparent. Stone walls tend to be three feet high or less. Wood fences are generally of the rail or picket type. Full closure of the edge, where it occurs, is with understory plant materials. The road edge is well kept but informal. One senses a well defined road way fully integrated into the rural landscape.

- Guidelines - A. The existing definition of the roadway edges, consisting of stone walls, fences, grade changes and mature trees is strong and is to be preserved and maintained.
- B. Other landscape treatments of the roadway edge are to be of an informal, natural character. Formal lawns, curbs, sidewalks, etc. are not appropriate. Where fences are used, they will be of a transparent rail or picket type and if painted, white.
- C. The roadway surface should be maintained as a rural road, high crowned and informally edged.



## MEAD STREET

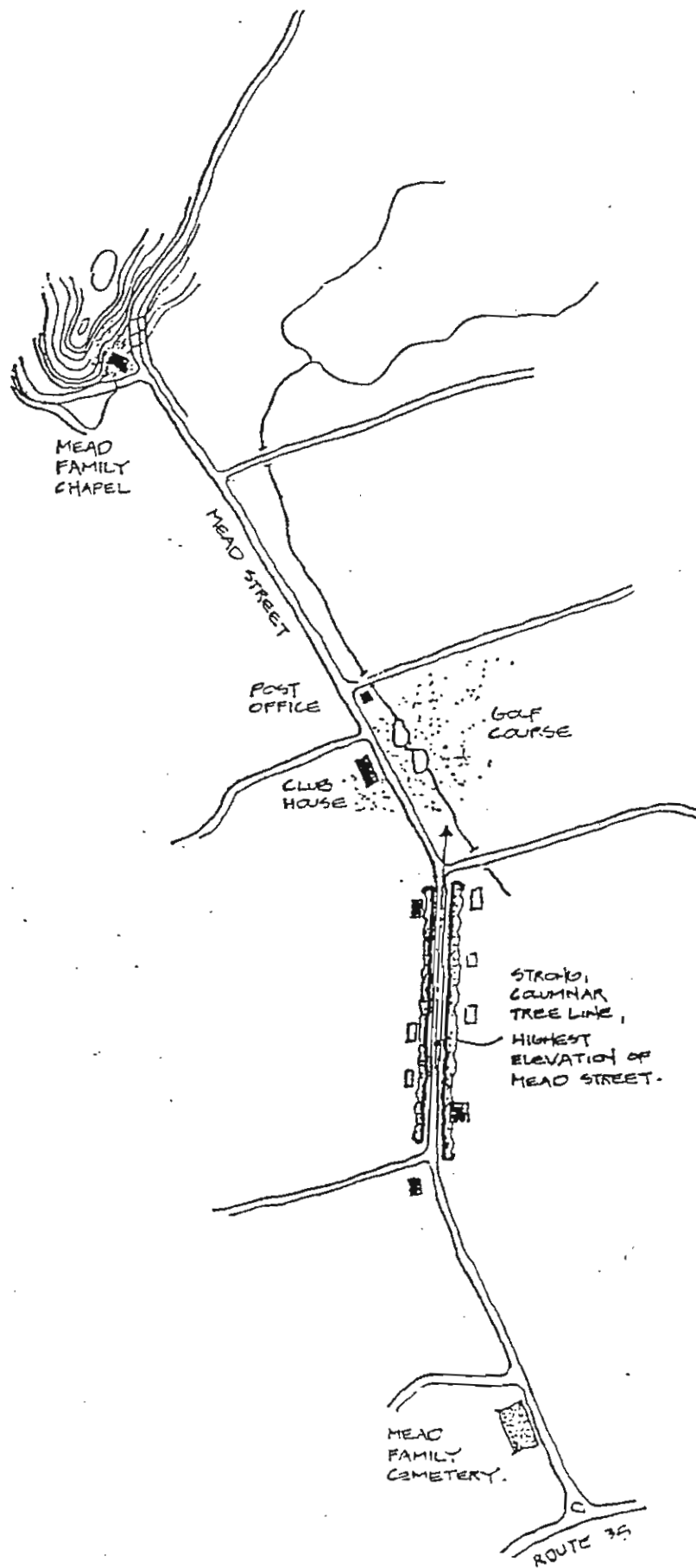
### 3. VIEWS/VISTAS

Mead Street views are of two kinds, the first occurs perpendicular to the road, where the landscape edge is transparent and opens to a meadow or vista beyond. These openings occur intermittently and offer relief from the otherwise strongly defined roadway corridor. Such views occur at Hunt Farm Road, at the golf course, and at Tarry-a-Bit Road where vistas open both to the west, and to the east over Lake Waccabuc. Up the hill, past the Mead Memorial Chapel, one further vista opens again down to the lake.

The second kind of view occurs down the length of the road, framed dramatically by the large, mature trees that line the street. One such view occurs looking north from the high point of Mead Street down towards the golf course. A view of the lush green fairways is framed by the strong colonnade of street trees. A similar view occurs at the long down slope looking north on Mead from the intersection at Post Office Road.

As important as the definition of the roadway edges, the views and vistas from Mead Street further integrate the road with the rural landscape.

Guidelines - A. The views and vistas defined above are essential to the special character of Mead Street and are to be preserved.



## MEAD STREET

### 4. SPECIAL PLACES

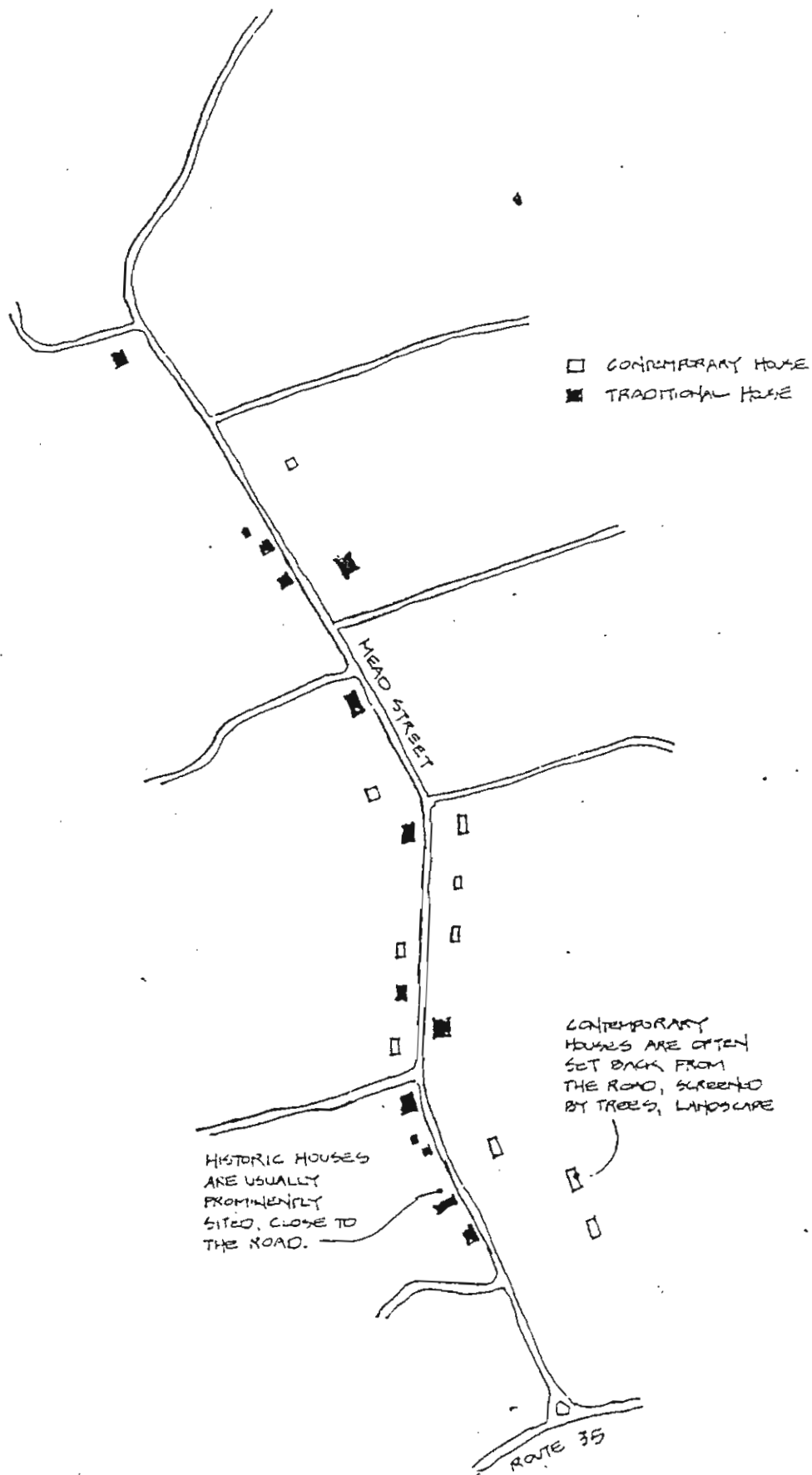
Special places are points of particular interest that occur periodically along Mead Street. They are special events or different uses that interrupt the residential pattern and add variety and character.

Proceeding north from Route 35, one first comes upon the Mead Cemetery, which in its own quiet way speaks volumes about the history of Mead Street. Beyond Schoolhouse Road, Mead Street reaches its highest elevation and begins a long down slope to the golf course. This stretch of road is defined by the most columnar street trees on the street, and is perhaps the most picturesque scene. Further north, the Waccabuc Country Club house and the Post Office are historically interesting buildings.

Finally, the Mead Memorial Chapel, a modest though impressive stone structure set in the hollow of a steeply rising ridge, and embraced by tall evergreen trees and masonry walls, punctuates the northern end of Mead Street.

Guidelines - A. The special places cited above are a part of the character and history of Mead Street and are to be preserved.





## MEAD STREET

### 5. HOUSES

Mead Street is an exclusively single family house residential area, but treatment of house style and siting is varied. While this variety is a part of the character of Mead Street, observation can teach which specific treatments work the best and are most consistent with the special character of Mead Street.

House styles - Mead Street houses fall into two categories:

Traditional - (19th and early 20th century)

Contemporary - (mid to late 20th century)

TRADITIONAL HOUSES on Mead Street have a direct and formal relationship to the street and a prominent entrance. The massing of these houses are simple forms, usually rectangular volumes, with pitched roofs.

Most of the traditional houses on Mead Street are two stories high with minimum roof pitch of about 6 in 12. These houses have architectural detailing and trim that reinforce and articulate the simple forms of the house. These elements include cornice trim, corner boards, window trim, etc. Outbuildings such as barns, toolsheds, and garages are usually detached, and are simple forms treated similarly to the main house.

CONTEMPORARY HOUSES usually have more complex forms quite different in character from traditional houses. They have lower more horizontal profiles and lower roof pitches. While often attractive and livable in the proper context, contemporary houses are usually inconsistent with the older, more traditional character of Mead Street houses.

## MEAD STREET

It is the traditional house that is the architectural component of the special character of Mead Street. The traditional house lends a sense of permanence and continuity that reinforces the timeless feeling of the landscape.

### HOUSE SITING

Houses on Mead Street, whether traditional or contemporary, are generally sited in two ways in relationship to the street:

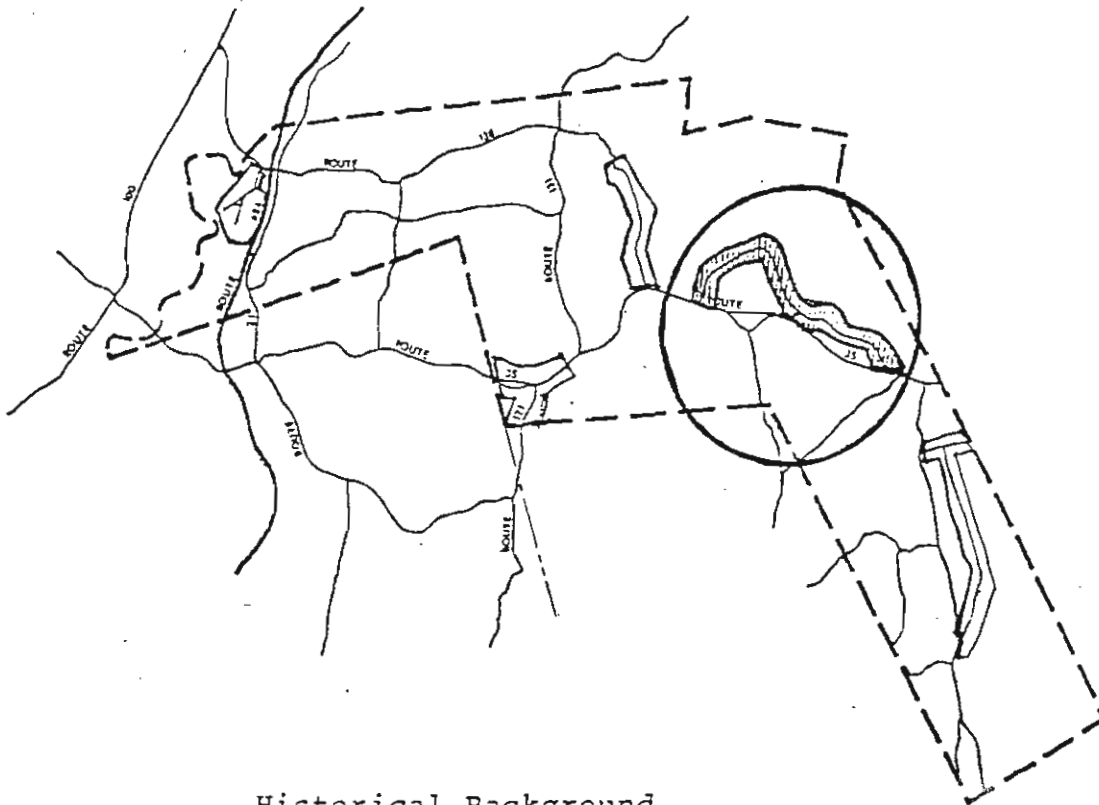
- Close to the street in prominent view.
- Set back from the street, screened by landscape planting.

Traditional houses pre-date the automobile, driveways and traffic, so that the entrance was directly on the street. These houses have a prominent and important presence on the street that does much to define its character.

Guidelines - A. Houses that are sited close to the road have an important visual impact on Mead Street. New houses that are to be sited in a prominent manner should be consistent in character with the traditional houses on Mead Street. These traditional houses will be the standard for architectural scale, proportion and detailing for prominently sited new houses.

- B. Houses that are sited away from the road and are screened by landscape, therefore having no visual impact on the character of Mead Street would not be subject to design criteria.

SOUTH SALEM  
SPECIAL CHARACTER DISTRICT



Historical Background

Town records begin in 1747 and 1764; there were 183 dwellings in the town of South Salem. Starting at the Meeting House (Presbyterian Church), one house after another brings back memories of the early settlers. The Keeler house, now the Town House, and the Wood, Lawrence, Lockwood, Studwell, Hoyt, and Bouton houses all have a story.

Perhaps the most famous was the Gilbert House. Although no longer standing, there is a stone in the wall on Main Street that reads:

"Behind This Wall Stood the Home of  
Jacob Gilbert In Which  
Major John Andre Was Held Prisoner  
September 24 and 25, 1780  
In This House He Wrote to Washington"

## SOUTH SALEM

Around the corner and up the hill on Bouton Road stands the house Captain Moses Bouton built upon returning from the Revolutionary War. It remained in the family until 1963.

Les Bouton (born 1889) and his sister Louise were the last to live in this house. It distressed Les to see so many old stone walls destroyed to make way for new developments. He remembered the long hours and hard work it took to clear the land and build the walls. Stone walls represent a work of art and a monument to history that should be preserved.

Les Bouton's lifestyle as a farmer no longer exists in South Salem, but the homes and stone walls still do.

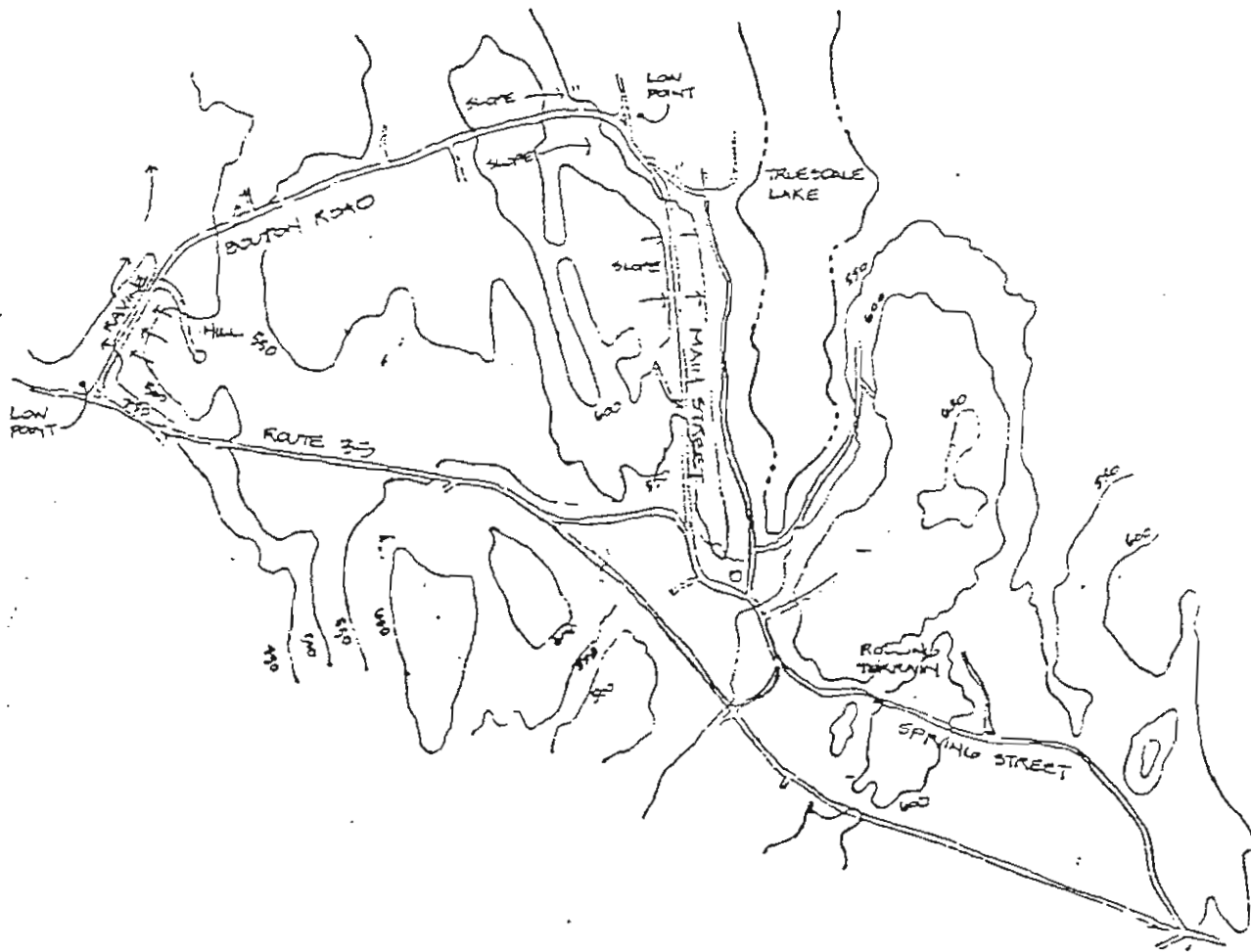
## SOUTH SALEM

### ANALYSIS OVERVIEW

South Salem is a special character district featuring a rolling, wooded terrain, a quaint village center, and a large stock of 18th and 19th century homes, many of them of an intimate, rural scale. The use is predominantly residential, although there is a pleasant mix of commercial and municipal services at the center of the village. Traffic is relatively low and is not a factor. The intent of these guidelines is to preserve the village character and scale of South Salem.

### GUIDELINES SUMMARY

1. Preserve the existing topography.
2. Preserve and maintain existing landscape features and roadway definition.
3. Preserve and maintain existing views.
4. Preserve and maintain the existing special places, especially the village center.
5. Alterations and new construction should relate closely to the scale and siting of existing South Salem houses.



## SOUTH SALEM

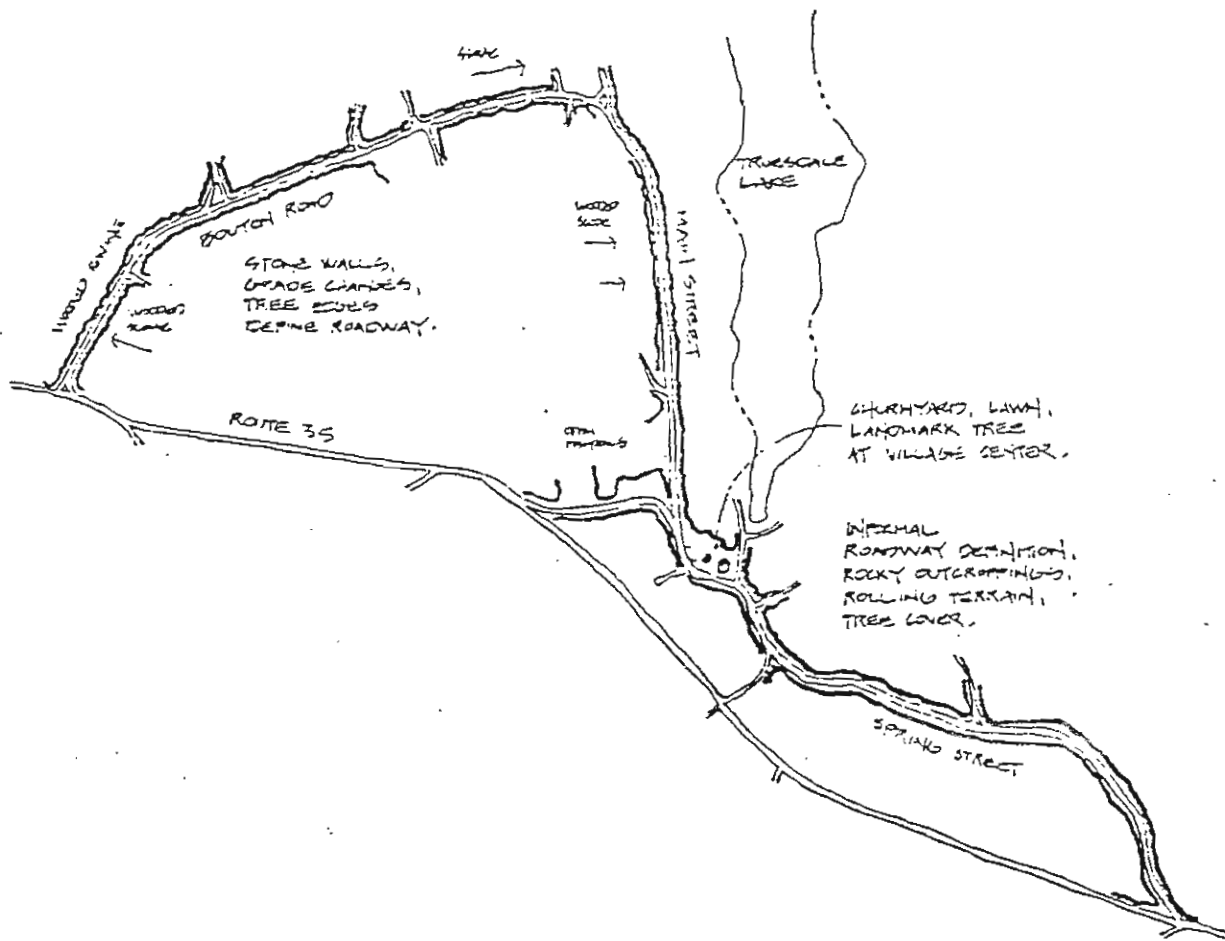
### 1. TOPOGRAPHY

The South Salem district is largely a wooded area with a gently rolling terrain. The village center sits at the south end of Truesdale Lake from which wooded hills rise gently to the east and west.

The low elevation of the district occurs where Route 35 meets Bouton Road. Bouton Road rises past a steep embankment on the right and a ravine on the left. Bouton continues to rise as it travels east, crests and then descends quite steeply to its intersection with Main Street. Main Street follows a fairly level course south, while grade rises to the west and falls to the east of the roadway. Main Street drops to its low point at the village center where it merges with Spring Street. Spring Street then leaves the village center, rising gradually as it travels east on a winding and rolling course to its terminus at Route 35.

Guidelines - A. The existing, natural topography of South Salem is an essential element of the character of the district and should be preserved.





## SOUTH SALEM

### 2. LANDSCAPE FEATURES/ROADWAY DEFINITION

The canopy of trees that covers the rolling terrain of South Salem is the district's primary landscape feature. The tree canopy, whether it consists of a simple forest edge or more formal street trees, combines with stone walls, grade changes and other elements to provide a strong, though varied, definition of the roadway.

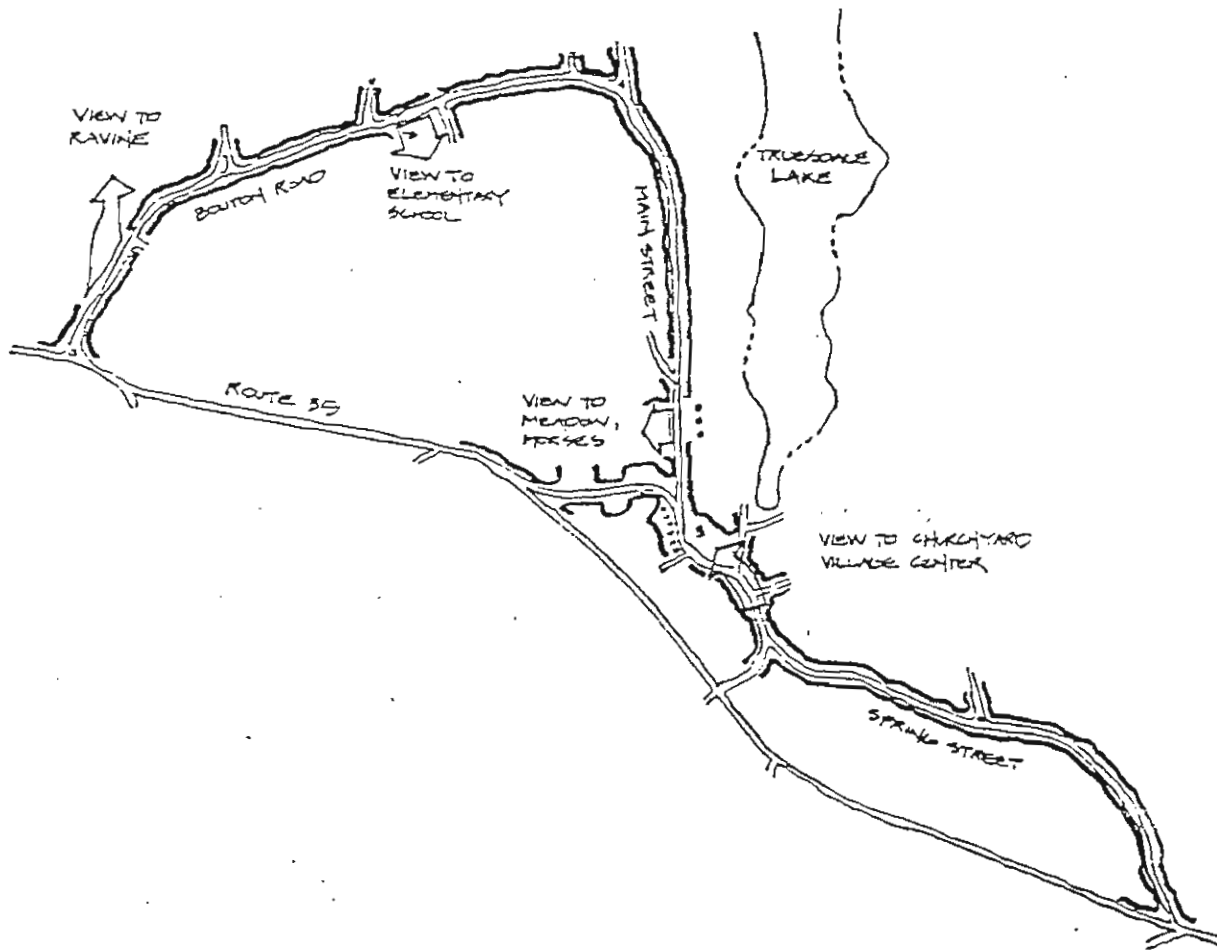
Bouton Road, at its beginning at Route 35, starts as a roadway strongly defined by forest edges, stone walls and grade changes. As Bouton Road continues the tree canopy opens up as houses occur on large lots, but the roadway definition via trees or lower scale landscape elements persists.

Main Street is heavily wooded and well defined except where newer houses to the east and below the elevation of the roadway occur. Elsewhere, strong tree lines and stone walls line the road.

The definition of Spring Street varies throughout its winding, rolling length. From Route 35 to the village center, its definition is via sporadic trees and stone walls, and is weak due to the general openness of this area. At the village center, Spring Street's definition is stronger due to the closely spaced houses and the strong presence of the sloping lawn of the South Salem Presbyterian Church. The balance of Spring Street is strongly defined by natural forest edge, rock outcroppings and grade changes.

Guidelines - A. The South Salem tree canopy should be preserved and maintained. Particular care should be extended to mature street trees that define the roadway edge.

B. Roadway edge elements (trees, stone walls, fences, grade changes) should be preserved and maintained and, where weak, strengthened.



## SOUTH SALEM

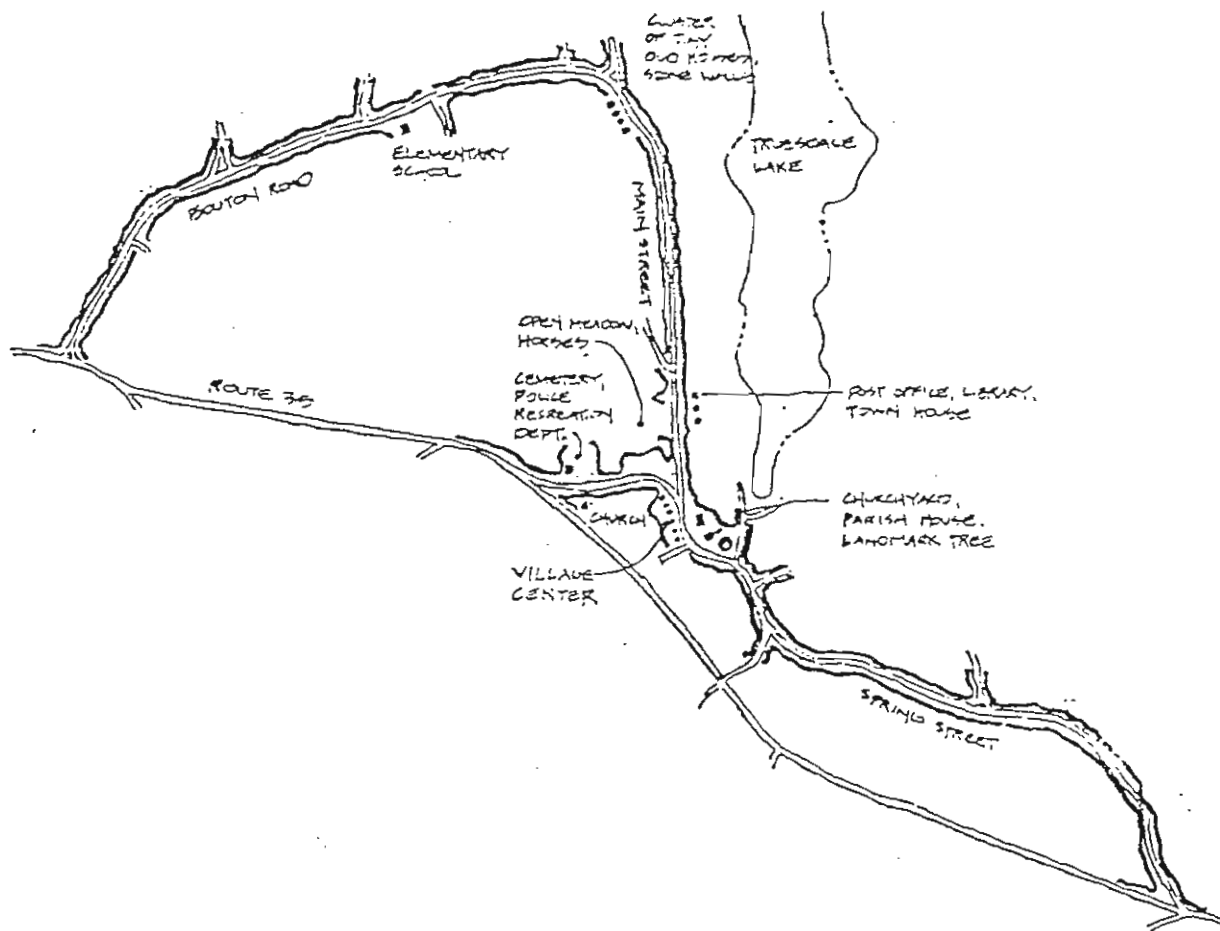
### 3. VIEWS

Bouton Road, Main Street and Spring Street are generally winding, rolling roadways, so views tend to occur perpendicular to the street. The tree canopy tends to obscure the long vistas, so the views are generally limited.

Views occur on Bouton Road just in from Route 35 where a ravine opens to the north, and at the Lewisboro Elementary School, where the view to the school opens, breaking the residential pattern and signaling a different, special use.

A view perpendicular to Main Street, opposite the Town House opens to a meadow where horses often graze. A good view of the Church and village center occurs as one enters the village from the east on Spring Street.

Guidelines - A. The above mentioned views should be preserved and maintained.



## SOUTH SALEM

### 4. SPECIAL PLACES

Special places are points of particular interest that occur periodically throughout the South Salem district. They are special events or different uses that interrupt the residential pattern and add variety and character.

Along Bouton Road, the Lewisboro Elementary School is a pleasant interruption of the otherwise residential street. Just beyond Bouton Road, on the west side of Main Street, a cluster of tiny, tightly spaced, historic 18th and early 19th century houses occur. They are close to the street and share a handsome stone wall at the roadway edge.

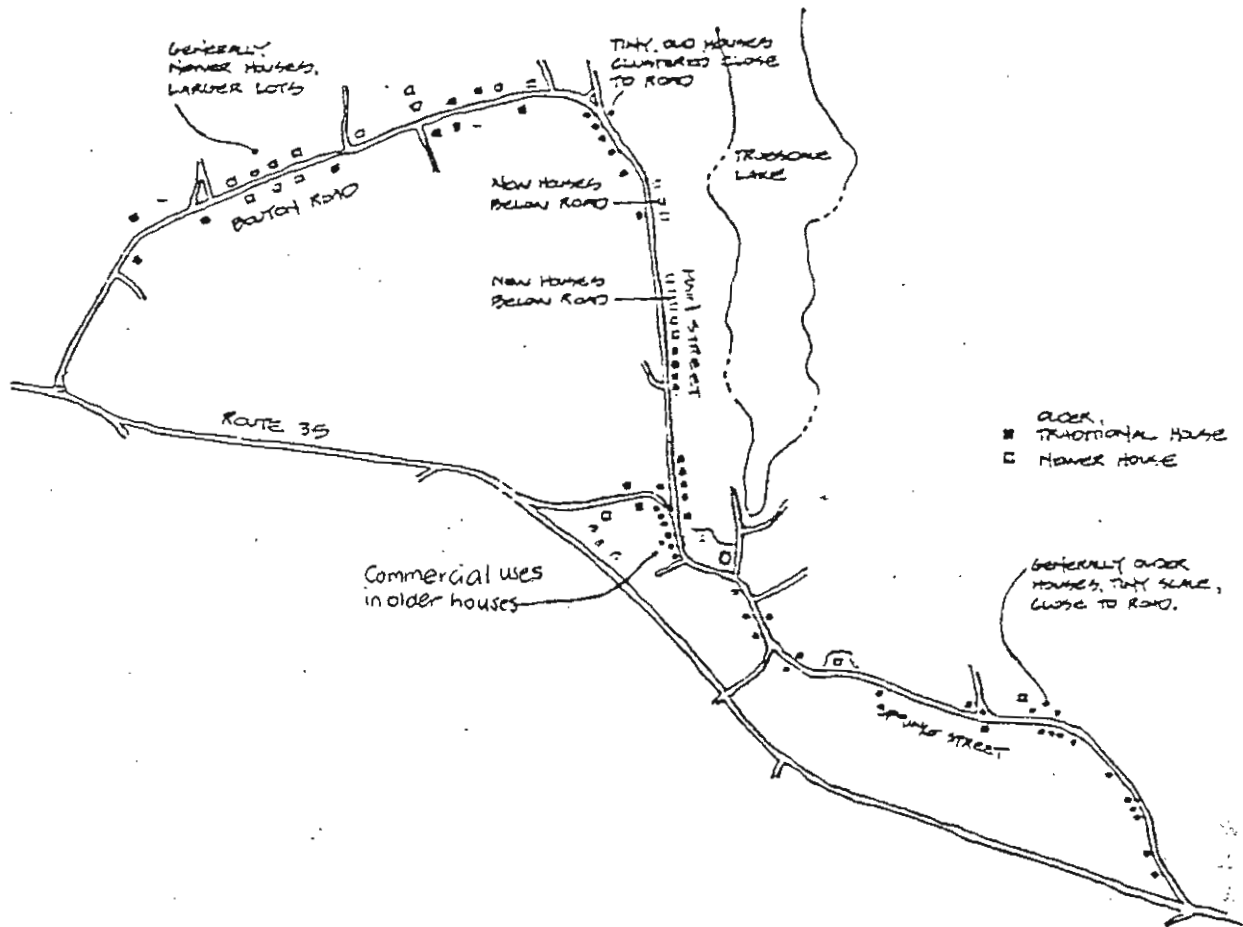
Further down Main Street are the Municipal buildings; the Post Office, the library, and the Town House. While built in different eras, the three buildings share common landscape treatments and paint color and thereby create a cohesive grouping.

Spring Street, as it comes from Route 35, is the entrance to the district. While the roadway definition of Spring Street is rather weak at this point, St. John's Episcopal Church, the cemetery, the Police Building and Echo Farm add considerable interest to this stretch of the road.

Finally, Spring Street turns the corner and enters the village center. Here, the village houses, the Church, the parish house, and churchyard, which features a large and ancient landmark tree, anchor the center of the South Salem Special Character District.

Guidelines- A. Preserve and maintain the above-mentioned special places.

- B. It is suggested that improvements to roadway definition of Spring Street from Route 35 to the village center, via street trees, stone walls or fencing treatments be considered.



## SOUTH SALEM

### 5. HOUSES

South Salem is a multi-use district, yet virtually all the buildings are of a residential character whether or not they are of a residential use. While there is a great variety of house types and styles in the district, there are pockets of remarkable cohesiveness of historical style and scale.

Bouton Road has the widest variety of houses in the district, many of them newer houses, and most of them on large lots. Interspersed are large, older farmhouses.

Main Street also has a variety of houses, but they fall in distinct pockets. First, at the corner of Bouton and Main on the west side (and mentioned in Special Places) is the closely spaced, small scaled cluster of 18th or early 19th century Federal style houses. This row is quite special.

Farther down Main Street are two groups of newer homes on the east side and well below the street elevation. These are simple, builder homes, but it is their siting and landscape treatment that are problematic. The sites of these houses, once wooded, were cleared for construction leaving bare lots with immature landscape treatments. Furthermore, the houses are set back from the road and have little road edge definition in contrast to other houses on the street. Landscaping or fencing needs to be developed there.

Directly to the south of the above mentioned houses is an older group of homes, which are 19th/early 20th century vintage. They are sited close to Main Street on the same elevation. This row extends down Main Street to the Post Office. It is quite cohesive in its rural Victorian characteristics, including simple frame construction, pitched roofs, front porches and modest ornamentation. They are also grouped close together.

These characteristics continue into the last group of homes on Main Street, those extending from the Town House to the village center, and those in the village center. Some of these are now in commercial use. This modest, rural Victorian house type defines the village center and, along with a touch of the earlier, late-Federal style, defines the architectural character of the district.



## SOUTH SALEM

### HOUSES (continued)

Homes on Spring Street, east of the village center, are spaced widely in a densely wooded area. They are also of 19th century vintage, but are most remarkable in their diminutive scale, usually one and one-half stories, and in their proximity to the street. Their intimate scale and closeness to the road make these houses quite special as well as rather fragile.

Guidelines - Alterations to existing houses should simply respect the integrity of historical style of the house. New houses or other new construction should be compatible with the scale, siting and materials of their close existing neighbors. Recommendations for specific areas within the district follow:

- A. Bouton Road- Houses on Bouton Road occupy open suburban type lots and tend to be large. New houses should be of similar size and massing, and have the same setbacks from the road and property lines as their immediate neighbors. The roadway edge should be treated as to continue existing forms of roadway definition.
- B. Main Street - From the corner of Bouton Road to the Post Office, Main Street is well wooded with good roadway definition by tree edge, stone walls or grade changes. New houses should preserve the tree canopy and roadway edge definition, and reinforce them if they are weak. New houses, if built near existing homes, should closely match the existing scale and siting. This is particularly important in the area of the Federal style houses on the west side of Main Street. From the Post Office to Spring Street, the houses are larger and the tree canopy more open. The same rules for siting and scale would apply.
- C. Spring Street - From Route 35 to the village center, the siting and scale of the houses and other buildings are quite disparate, establishing no particular pattern. As mentioned above, effort here might be best spent in strengthening the definition of roadway.

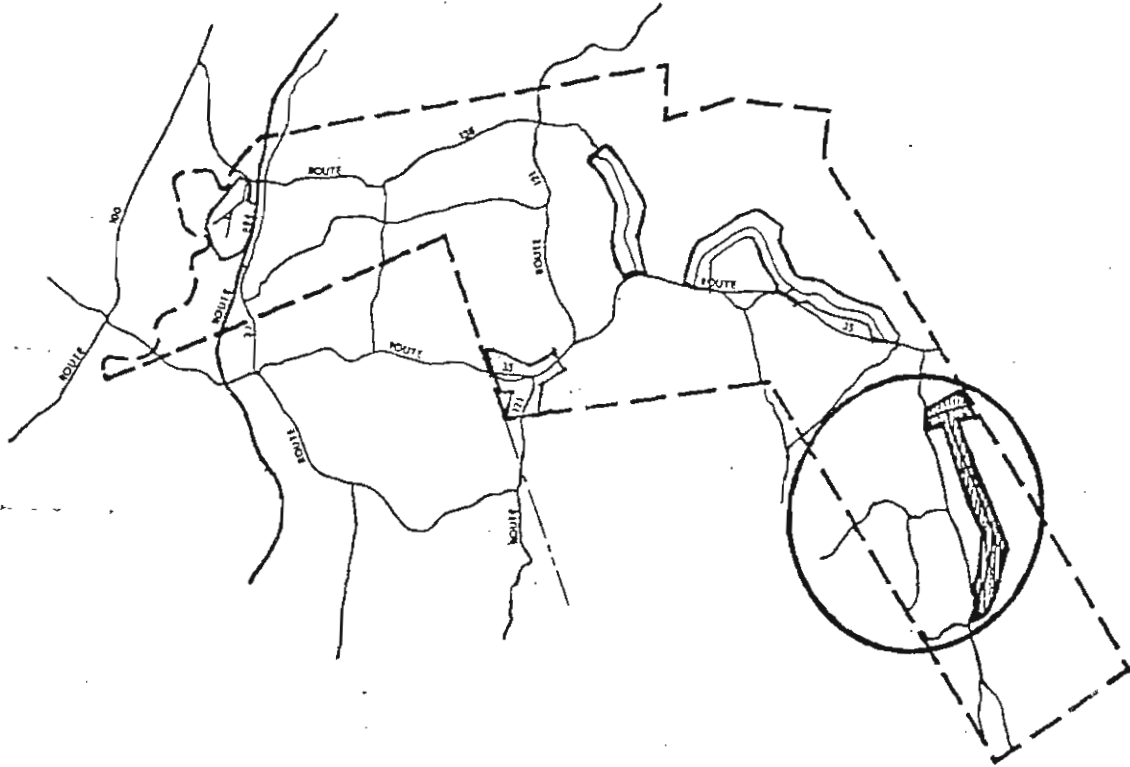
## SOUTH SALEM

### HOUSES (continued)

In the village center, closely spaced houses form the village commercial hub. Their closeness and uniformity of setback and massing form a street wall that, in concert with openness of the churchyard across the street, give the village center a strong sense of place. The setbacks and massing of these houses is to be preserved in any alteration or new construction. Furthermore, the appearance and detailing of these existing houses (frame construction, wood clapboard, double hung windows, etc.) should be used as a guide for alterations and new construction.

On Spring Street east of the village center, it is suggested that the tiny scale, road hugging character of the existing houses be respected by either new houses of similar scale and siting or by locating houses set back from the road, screened by the existing tree canopy and road way edge definition.

ELMWOOD ROAD  
SPECIAL CHARACTER DISTRICT



Historical Background

In 1840, one John Lewis donated \$10,000 to the Town of Salem with the intention that the interest from the fund be used for the common schools and that the Town be named after him, which it was. John Lewis was born in the house on the corner of Route 123 and Elmwood Road. Speculation is that the area, which includes Elmwood Road, became known as the Hamlet of Lewisboro because this was Lewis' birthplace and he had been a Town benefactor. People came to see this Hamlet as a residential farm area only.

## ELMWOOD ROAD

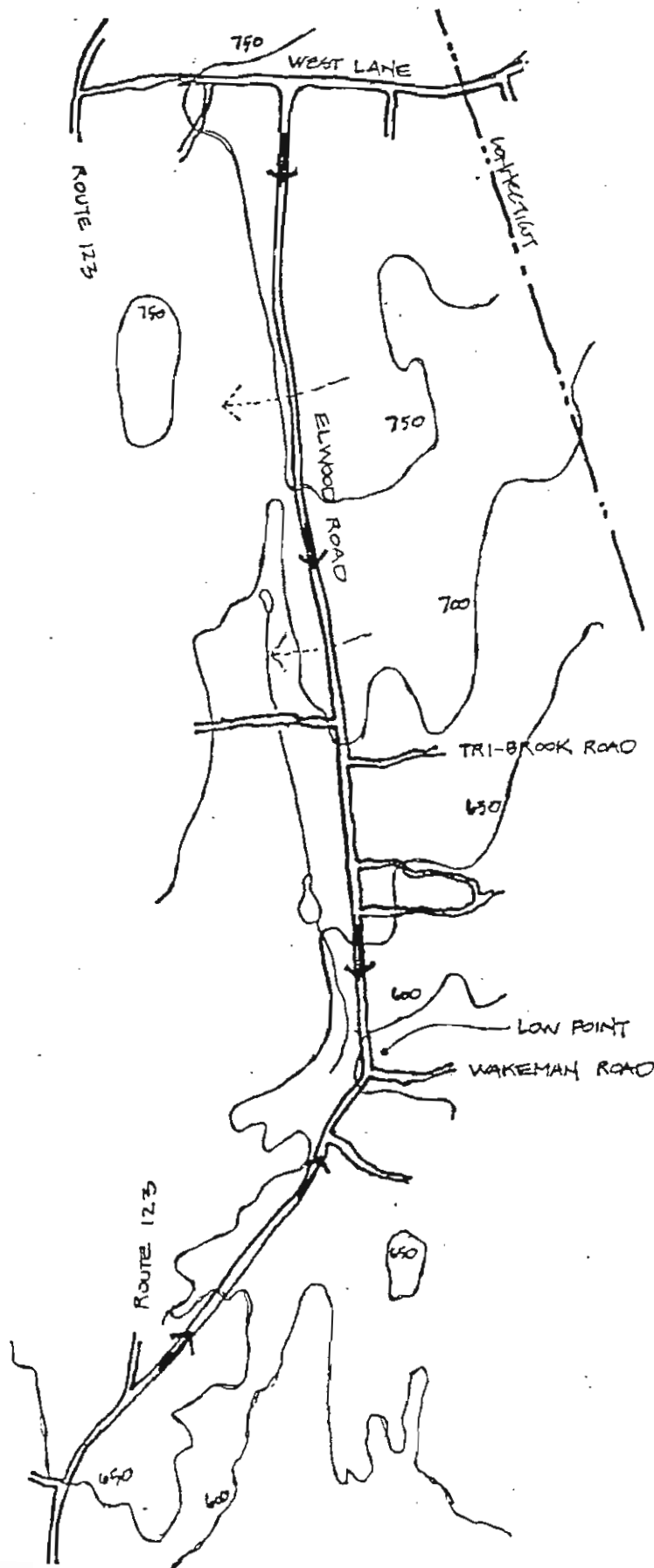
The mid-century population explosion changed the rural farming community until today there remains only one working farm. Many of the beautiful old farmhouses remain, one being Onatru Farm, located on Elmwood Road. Although no longer a working farm, it has all the necessary elements of one that could be maintained for posterity. In fact, that was the donor's wish. Alice Poor wanted to give her property to preserve the rural atmosphere.

## OVERVIEW

Elmwood Road is a Special Character District with a predominantly residential use in a naturally beautiful wooded, watershed area. Houses are integrated into the wooded landscape and set back from the strongly defined roadway corridor. The intent is to preserve these special Elmwood Road characteristics.

## GUIDELINES SUMMARY

1. Preserve the existing topography/landscape.
2. Preserve and maintain the existing roadway edge consisting of mature street trees, and/or forest edge, stone walls and grade changes.
3. Preserve the existing views and vistas.
4. Preserve the existing special places, including West Lane, Onatru Farm and St. Paul's Chapel and Cemetery.
5. Houses built new or altered will maintain or enhance their natural landscaped settings.



## ELMWOOD ROAD

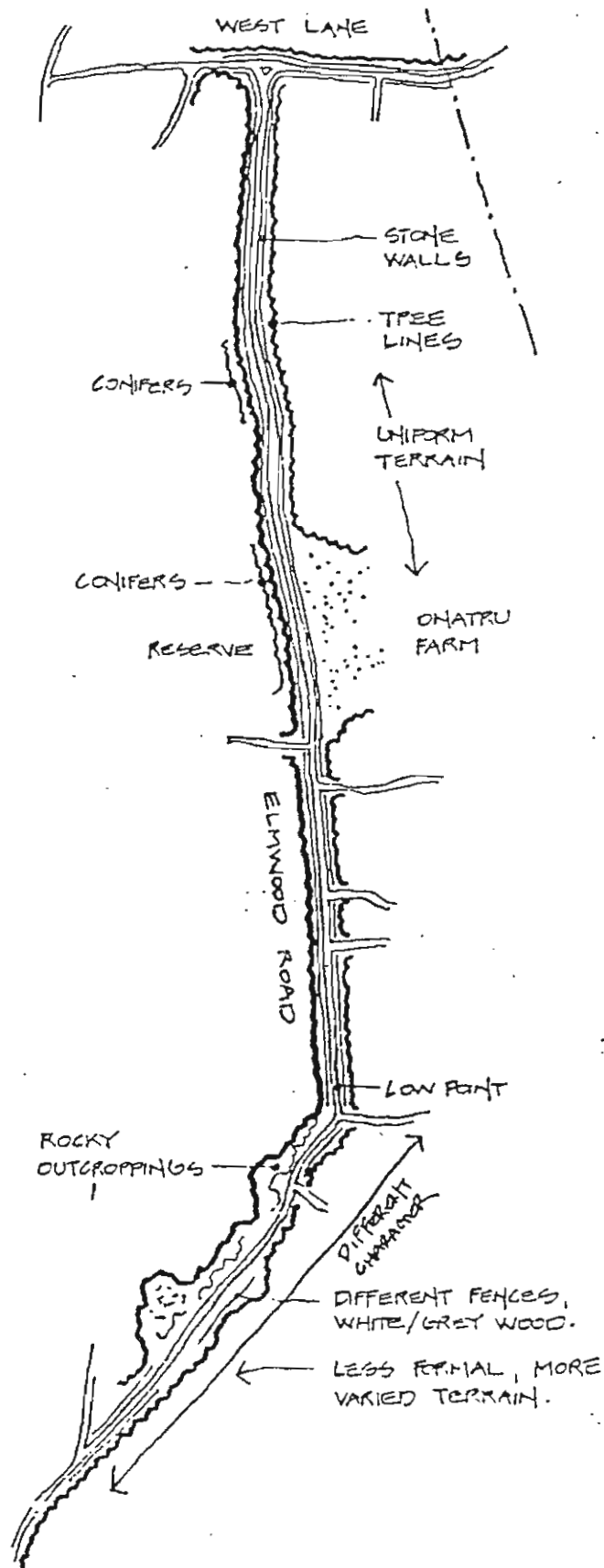
### 1. TOPOGRAPHY/LANDSCAPE

The Elmwood Road District is largely a wooded, watershed area, with a gently rolling terrain. Some higher, flatter areas are more open farmlands.

West Lane, at the northern end of the District, rises from either end to a high point near the intersection with Elmwood Road. Elmwood Road then begins a long, gentle descent. Grade is generally higher on the east side of Elmwood, sloping to a watershed and stream paralleling Elmwood to the west. Open areas occur on the higher ground to the east of Elmwood, while to the west it is consistently heavily wooded.

Elmwood reaches its low point at Wakeman Road, where the parallel stream passes under Elmwood and descends to Scott's Reservoir. Grade then begins to rise toward the Route 123 intersection. This stretch of Elmwood, also heavily wooded, has a rougher, rockier terrain.

- Guidelines - A. Elmwood Road's natural undisturbed quality is its strongest characteristic. The tree cover and sparse pattern of development through the district should be maintained.
- B. Existing grades and patterns of drainage should not be altered.
- C. Mature trees, whether individual street trees or at forest edges, should be preserved and maintained.





## ELMWOOD ROAD

### 2. ROADWAY DEFINITION/EDGES

Both West Lane and Elmwood Road are strongly defined on both sides by a combination of stone walls, grade changes and trees. The trees are either formally placed mature street trees, or a more natural forest edge. In either case, the sense of roadway corridor is strong, and the separation of public street and private domain is clear.

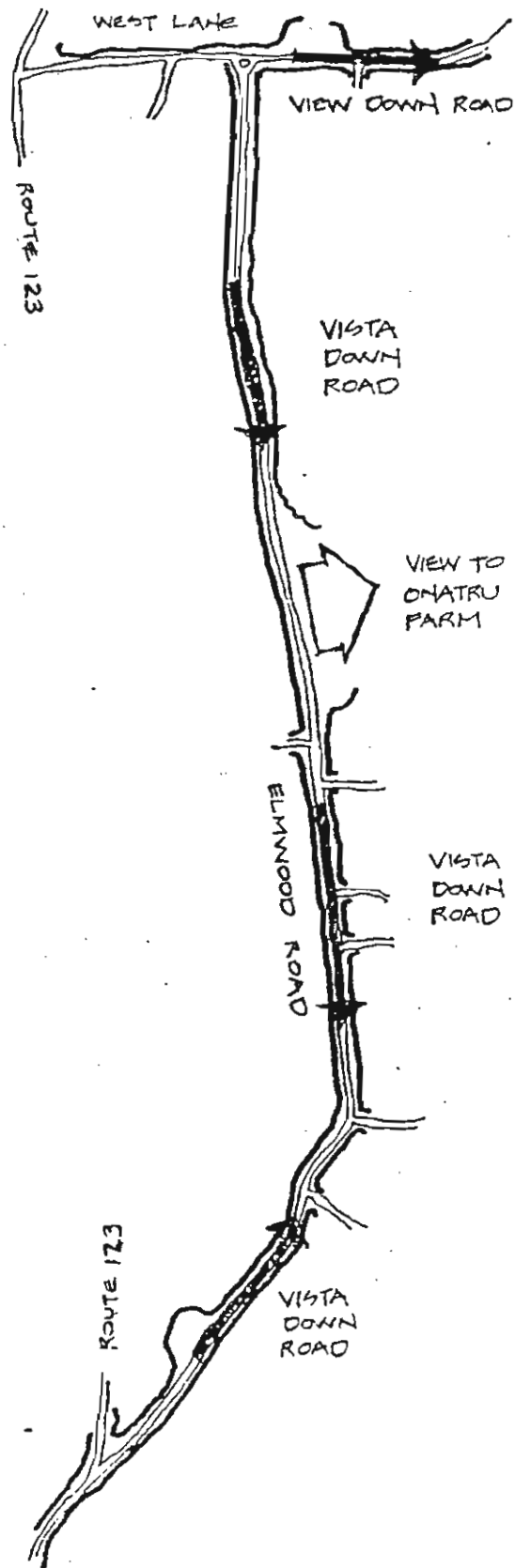
South of Wakeman Road, where the terrain is rougher, the roadway edge definition is more varied, including rocky outcroppings, understory plantings, and fences.

Overall, the interaction of these defining elements gives Elmwood a general impression of a roadway passage through a forested natural area.

Guidelines- A. The existing definition of roadway edges, consisting of grade changes, stone walls and either formal street trees or informal forest edges, is to be preserved and maintained.

B. Other landscape treatments of the roadway edge are to be of an informal, natural character. Sidewalks, curbs, formal lawns, etc. are not appropriate. Where fences are used, they will be of a generally transparent rail or picket type.

C. The roadway surface should be maintained as a rural road, high crowned and informally edged.



## ELMWOOD ROAD

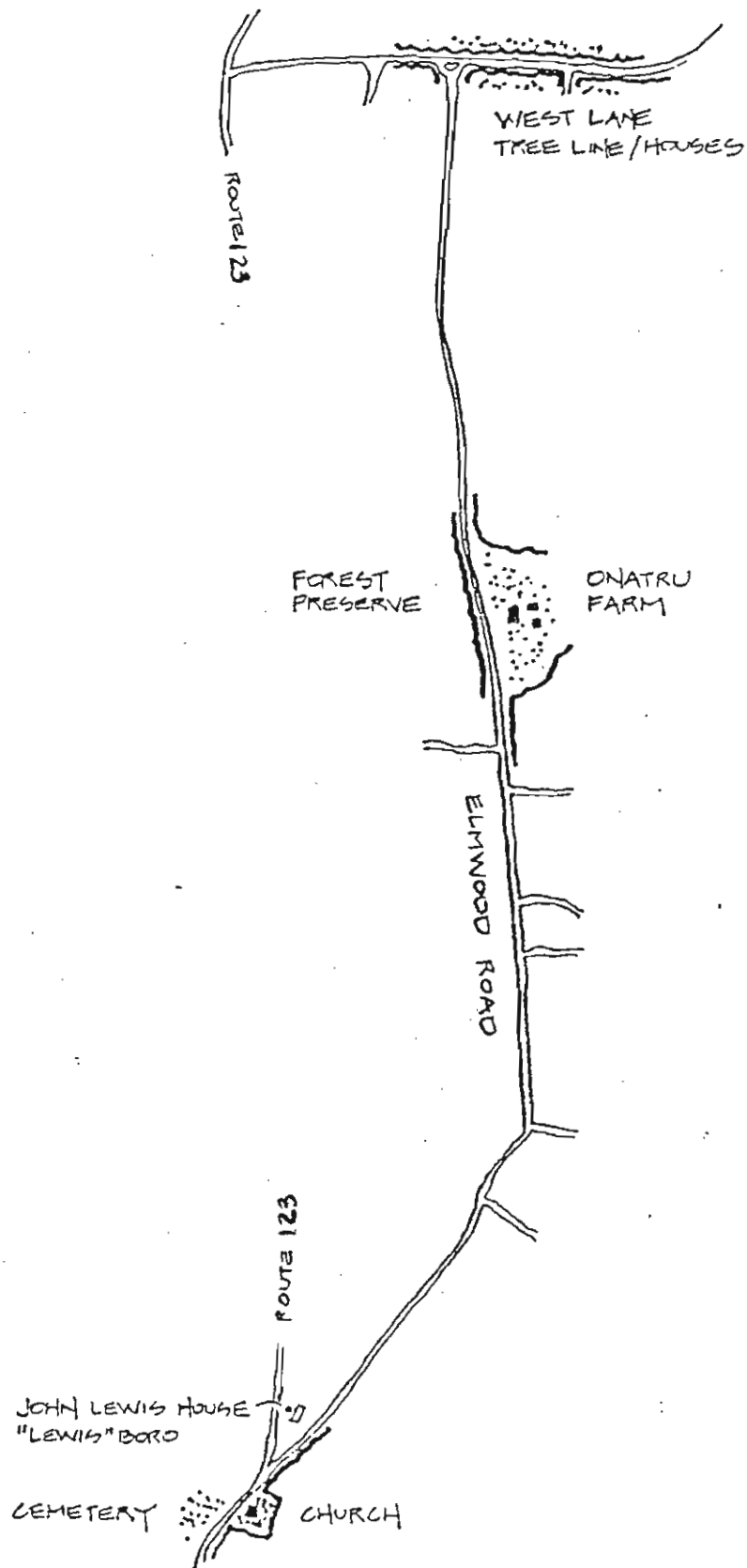
### 3. VIEWS/VISTAS

The Elmwood Road district is predominantly forested, so that most views occur down the length of the roadway corridors. Such views can be observed on both West Lane and on the northern portions of Elmwood. These views are strongly formed by the trees, stone walls and grade changes that define the roadway edge.

One view perpendicular to Elmwood opens up to the east of the road at Onatru Farm, where the tree cover gives way to open meadow.

The views up and down Elmwood Road are an important element of the character of the district.

Guidelines - A. The views and vistas defined above are essential to the special character of Elmwood Road and are to be preserved.



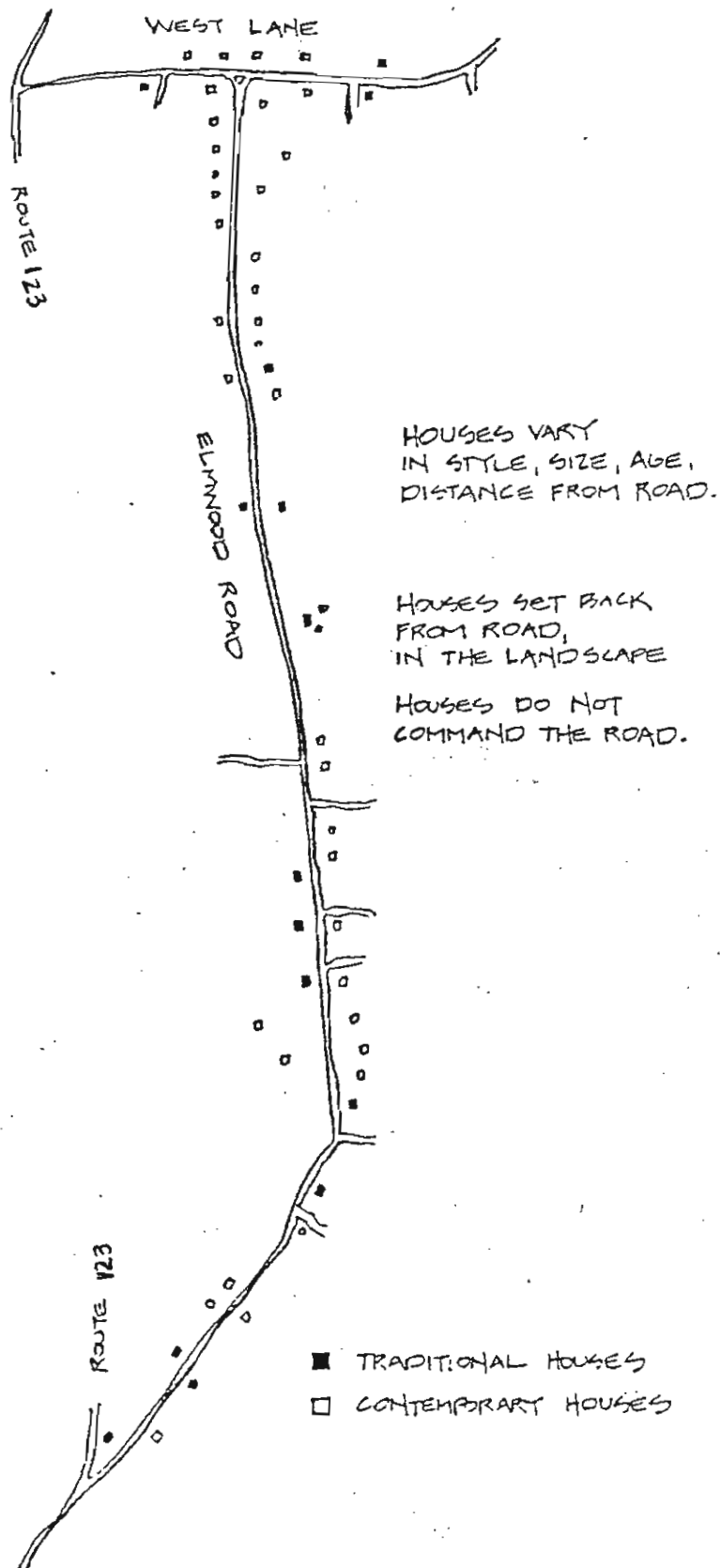
## ELMWOOD ROAD

### 4. SPECIAL PLACES

Special places punctuate the ends and the middle of the Elmwood Road Special Character District.

At the northern end of the District, West Lane from the Connecticut state line to the Elmwood Road intersection is of particular interest. Columnar street trees give this area a stately formal character. Midway down Elmwood Road, Onatru Farm and the forest preserve are attractive park and special activities areas in this otherwise residential district. Finally, the John Lewis House, St. Paul's Chapel, the cemetery and the parish house at the intersection of Route 123 mark the terminus of the Elmwood Road District.

Guidelines- A. The special places cited above are part of the special character of Elmwood Road and are to be preserved.



## ELMWOOD ROAD

### 5. HOUSES

Elmwood Road is a predominantly single family house area. While the district has a wide variety of house types and styles, there is nonetheless a coherence about Elmwood Road that belies the apparent dissimilarities of its parts.

The houses on West Lane and Elmwood Road run the full gamut of style, from early 19th century farmhouse to late Victorian gingerbread, 1950's ranch and contemporary builder homes. However, most houses are similar in a number of ways regarding the siting and landscape treatments.

First, the lot sizes are large (2-4 acre zoning) and the houses, with the exception of a few shallow lots, are set back a distance from the road, and from their adjacent neighbors.

Second, on most lots, enough of the natural tree cover remains to preserve the sense of a naturally forested landscape. In addition, the houses generally have landscape treatments, including foundation plantings, that further integrate them into the natural setting.

The result of the setbacks and the integration of the houses with the landscape is that the houses tend not to have a large presence on the road. Rather, it is Elmwood Road itself, with its defined but natural edges and woodland feeling, that best defines the special character of the district.

Guidelines - It is the intent that the construction of new houses or alterations to existing houses and properties will achieve the same integration with the natural landscape as now generally exists in the Elmwood Road Special Character District.

- A. New houses will be sited well off the street line and property lines, in no case closer than their immediate neighbors. Existing trees that make up the natural tree cover will be preserved wherever possible. New houses will have foundation and other plantings as to best integrate the house into the natural landscape.
- B. Alterations to existing houses are encouraged to fit with the style of the existing houses and to maintain or enhance the landscape setting of the original house.

APPENDIX A

Town of Lewisboro Master Plan, 1985

Excerpts regarding Special Character Districts

Section VI Community Character and Appearance  
Pages 120 - 133

The maps included in this excerpt of the Master Plan, prepared by Frederick P. Clark Associates, Planning Consultants, denote the delineated boundaries of the Special Character Districts, as well as the existing uses.



# Lewisboro Town Master Plan

Town of Lewisboro  
Westchester County, New York

Adopted by the Town of Lewisboro Planning Board  
May 24, 1985

## VI. COMMUNITY CHARACTER AND APPEARANCE

The preceding chapters of this Plan discuss three types of land use and the provision of public services and utilities. Recommendations are made as to the best use of land to achieve the goals expressed in the first chapter. However, simply having the most appropriate uses in the most appropriate locations will not itself ensure that the Lewisboro of twenty years hence will appear the same as the Lewisboro anticipated by this Plan.

This chapter proposes that the key to realizing this Plan's vision of the future Lewisboro is, first, to identify the best qualities of the community's character and overall appearance and, second, to commit to maintain and enhance those qualities. This approach applies to every type of land use, development project and public facility.

### A. Residential Areas

Even after experiencing substantial residential growth, Lewisboro continues to have a visual identity distinct from that of neighboring towns. The higher density areas are well separated from each other and major roads by wide areas of wooded lands which conceal more isolated residences. The view south from Lookout Point in Westchester County's Mountain Lakes Camp on the North Salem border is one of seeming forest with only the homes on the edge of Lakes Waccabuc, Rippowam and Oscaleta revealing the presence of a residential community. A similar view is seen from other high points in Town.

Of course the present forested view of Lewisboro is quite different from the days when the Town was an active farm community - in a very real sense the vegetation has taken over. Beneath the undergrowth are the stone walls that delineated growing fields, pasture lands, swamps and farm borders. Now often hidden behind trees are numerous homes from the nineteenth and late eighteenth centuries. Fourteen historic cemeteries dot the landscape. The heritage of Lewisboro is far from lost.

Steps can be taken to insure that the same statement can be made in 2005. Of prime importance is the recommendation of the residential development chapter that the low density residential character of much of Lewisboro be maintained through application of rural and low density development standards. Additional guidelines can be followed regardless of development density. These are:

1. Use of Natural and Man-Made Features. The major features of the natural and man-made terrain should be incorporated into subdivision and site plan design whenever possible. This guideline has long been applied by the

Planning Board but stronger legislative support should be considered. Features of specific concern include:

- a. Intact stone walls should be preserved and used for property line definition when reasonable.
  - b. Natural wetlands and stream corridors should remain undisturbed by new construction or lot development.
  - c. Steeply sloped land should be protected from destructive encroachment.
  - d. Wooded areas should retain their character.
  - e. Remaining open space fields should be maintained for their own unique contribution to the landscape. Consideration should be given to selective clearing where it would serve a landscape architectural purpose or enhance a historical setting.
2. Separation From Major and Collector Roads. All new construction should continue the existing pattern of much of Lewisboro's residential development by being separated visually from major and collector roads. The rural atmosphere of the community is enhanced by open space or green corridors on the most traveled roads while at the same time new homes are best oriented toward quiet neighborhood streets.

In some circumstances, particularly in the hamlets, it may be appropriate to have new construction front on collector roads. In these situations, the new residences should be integrated with and related to the existing residential character. The use of high solid fences to screen such homes from the roads should not be premitted where it would create a tunnel effect along the road and defeat the green corridor or hamlet concepts.

3. Street Construction. New residential streets should be designed to fit in with the natural landscape. Major grading, excavation, excessive rock cuts and stream/wetland disturbance should be avoided. Limited relaxation of standard design guidelines should be considered as a means to mitigate environmental disturbance when no other option is available.
4. Architectural Review. Single development projects which involve the construction of a large number of residential units within a short period of time can have a dramatic impact on community appearance due to their scale alone. Detailed consideration must continue to be given by the Town to this aspect of any such development. As it has begun to do, the Planning Board should review large single-family subdivisions for architectural variety and

avoidance of repetition in building and site design. Multi-family developments should continue to be subject to more comprehensive architectural review.

## B. Commercial Areas

In 1984, each of Lewisboro's four hamlets encompassed small business areas. The commercial development chapter of this Plan recommends that these areas remain neighborhood business centers. Within this framework, the Plan designates room for growth which is directly related to the likely needs of the Town's present and anticipated future residents. While this commercial growth is both necessary and recommended by the Plan, it could affect the quality of the environment if not carefully reviewed by Town officials. Guidelines which should be applied to all commercial development for the purpose of protecting and enhancing community character and appearance are:

1. Visual Appearance. A visual cohesiveness should be promoted in each hamlet. This requires that all new buildings relate architecturally to existing buildings. It also requires that the visual relation between buildings and road corridors be considered. Establishment of a sense of cohesiveness can best be achieved through strengthening of the architectural review process now conducted by the Planning Board for all new commercial buildings.
2. Circulation. The visual integrity of the hamlets can be further enhanced through the development of a functional circulation system. At the time of site development plan review, vehicle and pedestrian links should be given careful consideration in terms of parking area connections, driveway locations, pedestrian access and sidewalks.
3. Site Development Standards. Detailed standards on elements of commercial site development are contained in the Town's zoning regulations. These include guidelines on such features as outdoor lighting, signing, landscaping, parking requirements and screening. It is critical that these standards be set forth in concise terms and be periodically updated as they provide the basic implementation tool for regulating the appearance of commercial areas.

## C. Areas of Special Character

The above discussion centers on the protection of a general "Lewisboro character" applicable to the entire community. There are several sections of the Town which embody particular aspects of this character in a visually striking manner. Each of these sections encompasses a relatively large land area

which contains a substantial number of contiguous buildings reflective of a period of Lewisboro history. Each is also characterized by well-defined features.

Five sections of Lewisboro are described below as areas of special character. Each one continues today to present a unique setting similar to a specific period of Lewisboro's past. This Plan considers it possible to both protect the significant characteristics of these areas and to allow for new development. Future generations of Lewisboro residents will greatly benefit from the Town's commitment to special preservation policies over the life of this Plan.

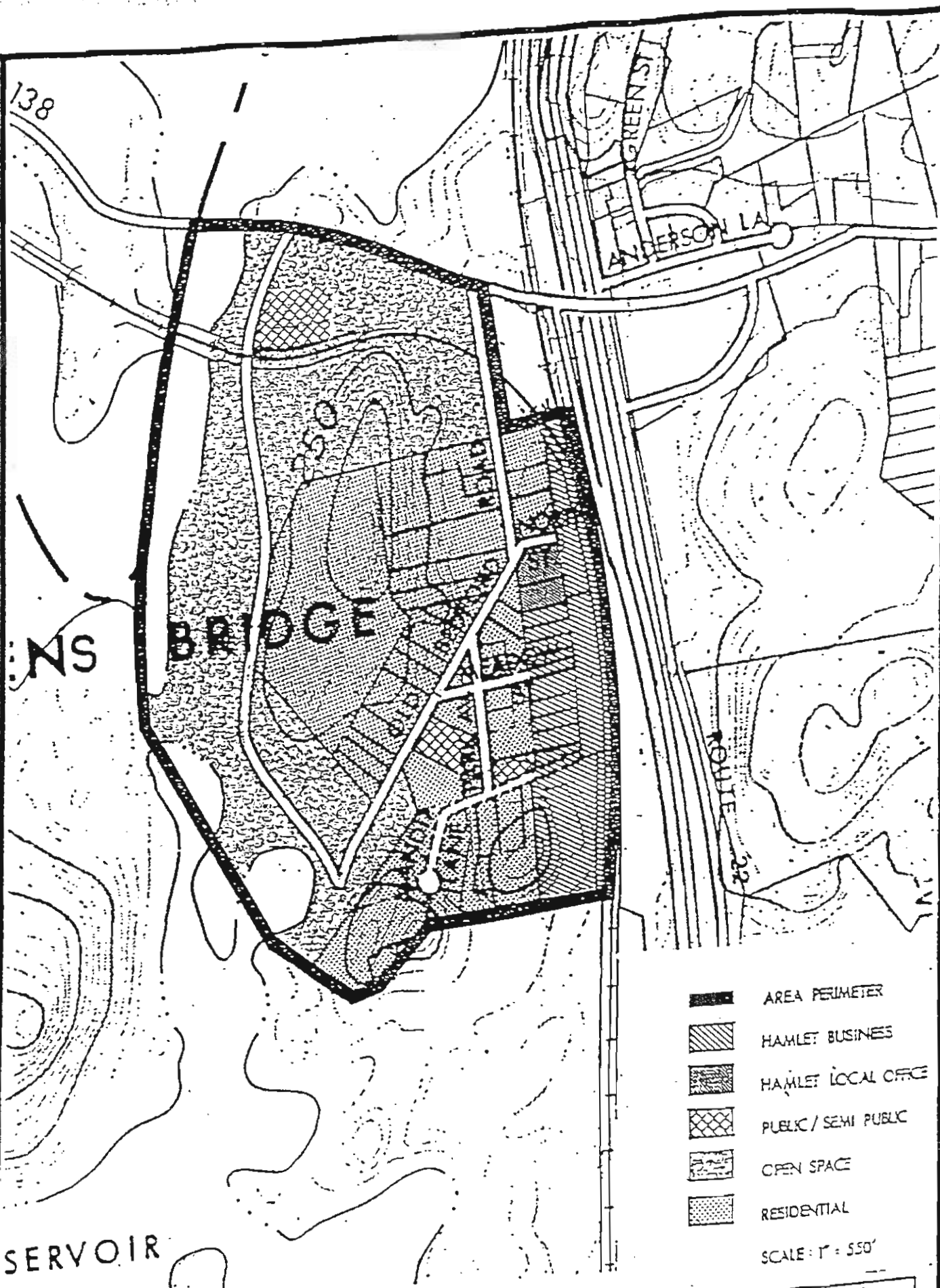
1. Old Goldens Bridge. Considering its history, it is a wonder that there is an Old Goldens Bridge remaining in 1985. Portions of the hamlet were subject to upheaval and destruction on several occasions including the permanent flooding of the Croton River as part of New York City's water supply program, the progressive widening of Route 22 and the construction of a six-lane Interstate highway. Through it all, a residential enclave reminiscent of a turn-of-the-century village has survived tucked between what is now a commuter railroad line on the east and what is still protected watershed land owned by New York City on the west.

Old Goldens Bridge, as outlined on Figure 11, contains many structures dating from the late 1800s to early 1900s. Access to the area is limited to Old Bedford Road which also serves the commuter rail station and several general business uses which line the railroad. Of special concern is the relationship between these commercial uses and the residential area. Preservation of the different period of time quality of this area is dependent on protection of the residential value of structures along Old Bedford Road and Park Avenue.

The following policies are established for Old Goldens Bridge:

- a. Commercial uses are to be confined to the land area presently occupied by such uses except as noted under "b" below.
- b. The two large old buildings (one being the former New York Store) located adjacent to the commercial area near North Street should be preserved.

Preservation can be encouraged by recognizing the potential for these buildings to be an effective buffer between the overtly commercial uses to the east and the residential area to the west. Hamlet Local Office uses, as described in Chapter III, would work well as a buffer and would also provide an eco-  
for continued building maintenance.



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

BRIDGE

ANDERSON LA

GREEN ST

ROUTE 22

SERVOIR


-  AREA PERIMETER
-  HAMLET BUSINESS
-  HAMLET LOCAL OFFICE
-  PUBLIC / SEMI PUBLIC
-  OPEN SPACE
-  RESIDENTIAL

SCALE: 1" = 550'

TOWN OF LEWISBORO  
TOWN PLANNING BOARD

Frederick P. Clark Associates, Planning Consultants

OLD GOLDENS BRIDGE  
SPECIAL CHARACTER  
AREA

  
NORTH  
FIGURE 11

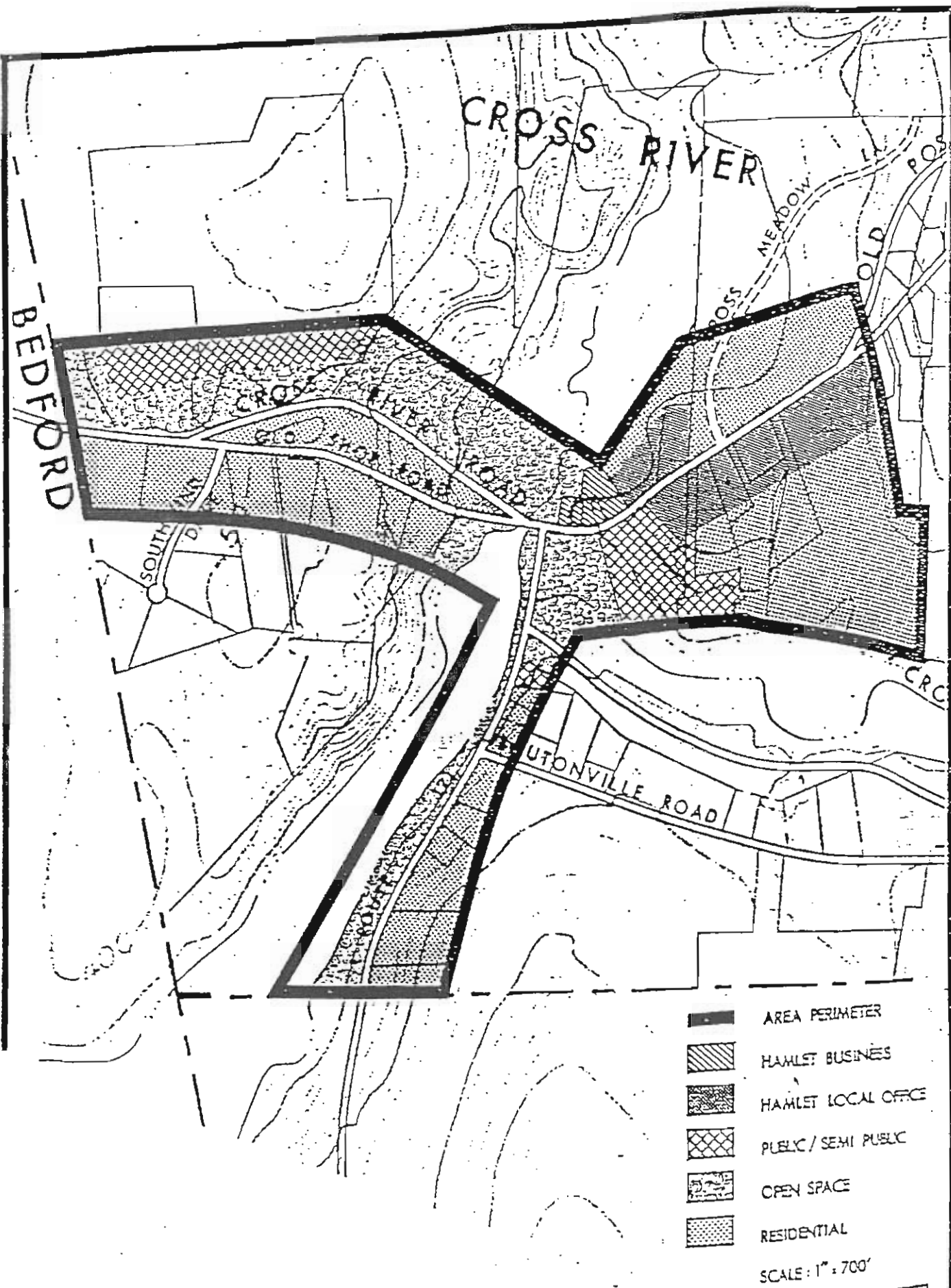
- c. All new construction proposed for Old Goldens Bridge should be subject to architectural review. New residential buildings should be found to be compatible with the area's architectural character and scale. New commercial construction within the existing business area should also be architecturally sensitive to the residential area and not be disruptive to the neighborhood.
  - d. Future residential development of the 10-acre tract located northwest of Old Bedford Road should be subject to the above review policies. In addition, all development should be buffered from the rear of existing residences fronting on Old Bedford Road.
  - e. The old railroad bridge which crosses the Muscoot Reservoir is listed on the National Register of Historic Places. It is an important community asset now mainly unrecognized. The structure itself, its setting and the old railroad bed extending to it from Old Bedford Road should be protected and enhanced. Any future expansion of commuter parking facilities or roads should not be permitted to infringe on this area.
2. Cross River. Some of Cross River hamlet's buildings have been facing the main roads through Lewisboro for over 200 years. Although the traffic has increased and some uses of property have changed, the visual image remains one of a nineteenth century village.

Figure 12 shows an outline of narrow corridors along Route 121 and Route 35 centering on their intersection at the Fifth Division Market. Within this perimeter is a remarkable density of historic homes, two church buildings, a cemetery and several retail stores. There is a cohesiveness to the area established by the age of the structures, the stateliness of the large residences and the low profile of the commercial uses. The challenge now is to maintain this antique quality with a mix of uses and increasing traffic volume.

The following policies are established for the Cross River special character area:

- a. All new construction proposed within the area should be subject to architectural review. Inappropriate design, bulk or setback could have a severe impact on the overall setting of the hamlet especially in recognition of the location of most properties directly on a State highway. The context of the entire area must receive priority in the review of individual plans.





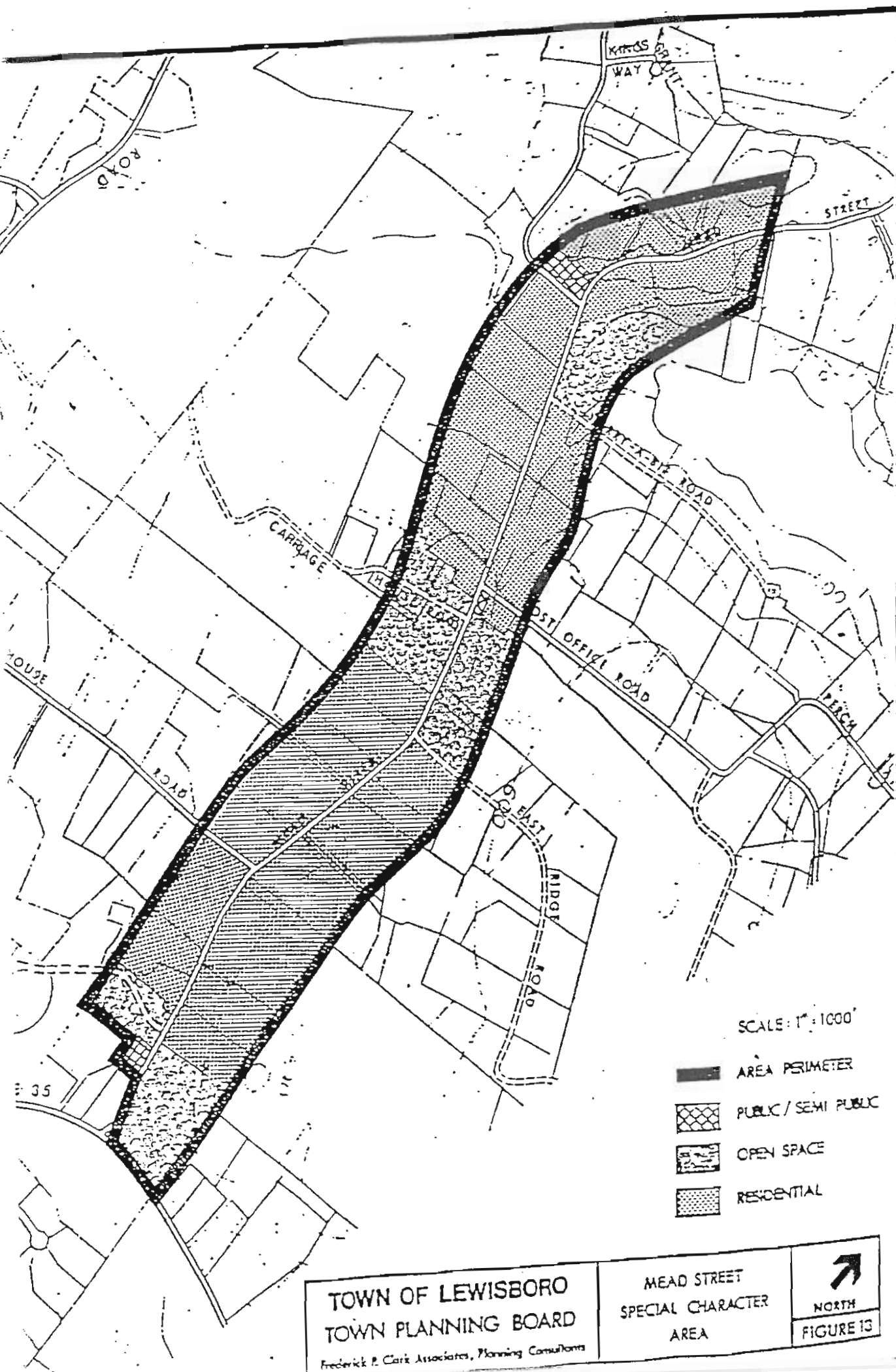


- b. The residential character of the area should remain dominant. To this end, the construction of buildings for commercial use should not be permitted.
  - c. Consideration should be given to permitting Hamlet Local Office uses, as described in Chapter III, in existing buildings which are located directly on Route 35 between Mark Mead Road on the east and Route 121 on the west. Subject to careful site plan review, this alternative use would both provide an economic incentive for preservation of these key structures and expand the range of professional services available within the hamlet to local residents.
  - d. Attention should be given to pedestrian movement through the Cross River area. At present, the lack of walkways makes leisurely strolling unsafe if not quite impossible. This reduces opportunities to appreciate the historic quality and setting of the area. A general walkway plan should be developed. Over time, through Planning Board review of site plans, the individual efforts of property owners and Town and community group programs, a walkway system could begin to emerge.
3. Mead Street. For two hundred years the Mead family has been building homes along this road which carries the family's name. Although in this century the Meads and their decendents have been joined on Mead Street by other families, the history of the old road is seen everyday in the quiet beauty of the street. The farms which were once dominant are gone but the stately old homes and well-tended lawns and meadows are still framed by trees and stonewalls.

There is a delicate balance between the scale of the homes, the road and the landscaping that exists for over a one mile length of Mead Street. Contributing to the overall setting are the Mead Street Burying Ground, the open spaces of the Waccabuc Country Club, the views of Lake Waccabuc and the Mead Memorial Chapel. Once lost, this special character could never be replaced.

The following policies are established for the Mead Street area as shown on Figure 13:

- a. Rural density residential land use is the only type of new development which is appropriate.
  - b. Architectural review should be required of all new construction. The existing character could be quickly diminished with the construction of an
- due to bulk, exterior finish



SCALE: 1" = 1000'

-  AREA PERIMETER
-  PUBLIC / SEMI PUBLIC
-  OPEN SPACE
-  RESIDENTIAL

TOWN OF LEWISBORO  
TOWN PLANNING BOARD

Frederick R. Carr Associates, Planning Consultants

MEAD STREET  
SPECIAL CHARACTER  
AREA

  
NORTH  
FIGURE 13

- c. Special attention must be paid to any proposal to develop the large tract located at the northeast corner of Mead Street and Route 35. The hillside on this tract faces the intersection and serves today as a pastoral gateway to Mead Street. All construction should be directed over the crest of the hill.
4. South Salem. "South Salem" is presently used as an address by over half of Lewisboro's residents, many of whom live far from the hamlet center. But for most, the name evokes a special image - a tree-lined village street, an imposing church on a hill, an old cemetery, antique homes both small and large and the Town House, itself once a stately residence. The roads leading into the hamlet - Spring Street, Main Street and Bouton Road, serve as portals to the old village center, lifting the traveler from the modern age of the State highways to the slower tempo of life along former country farm roads. Lining the roads are a remarkable assortment of historic buildings, some dating back to the eighteenth century but whose age is not necessarily apparent in a quick glance.

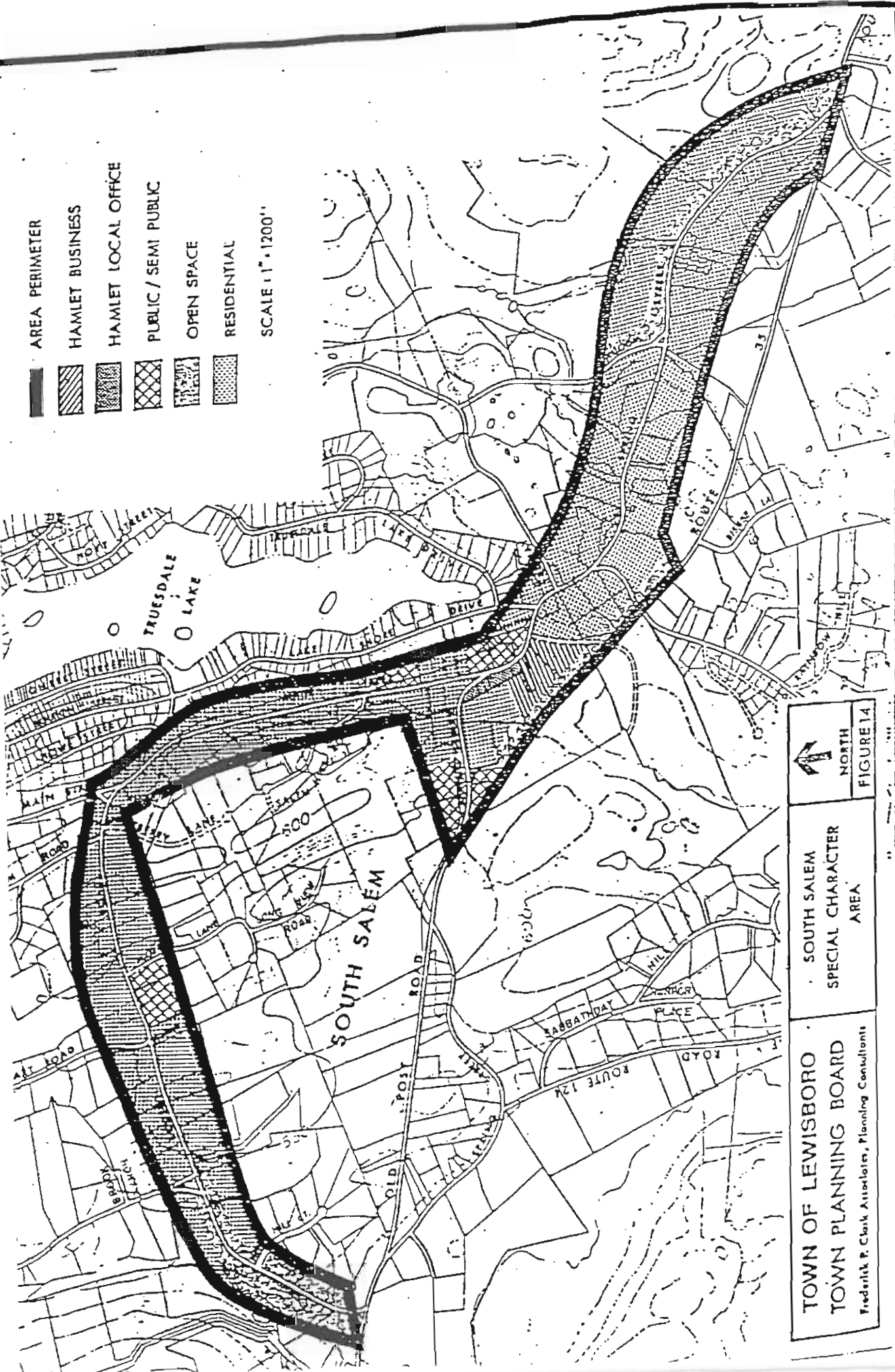
The center of South Salem and the roads leading into it establish an important community identity. This identity of a link to the farm community of the past should be protected for the benefit of all Town residents who travel through the area regularly.

The following policies are established for the South Salem special character area as shown on Figure 14:

- a. For all but a small part of the area, residential development is the only type of new development which should be permitted. The density of new development should be as shown on the Plan Map and differs by location.
- b. The cluster of buildings which line the west side of Spring Street across from the Presbyterian Church, north of the Salem Market, should continue to be used as residences. Conversion to the types of uses included under the Hamlet Local Office designation may be suitable but the expansion of existing parking to serve more typical commercial uses would destroy the attractiveness of the area. The buildings now occupied by an antique shop and the Horse and Sound restaurant are suitable for such limited commercial use.
- c. The approximately 5-acre area which fronts on Spring Street east of the Salem Market and extends through to Route 35 is designated by this Plan for long-term development as a Hamlet Business site. Any construction must be designed to complement and enhance the character and maintain a separation

- AREA PERIMETER
- HAMLET BUSINESS
- HAMLET LOCAL OFFICE
- PUBLIC / SEMI PUBLIC
- OPEN SPACE
- RESIDENTIAL

SCALE 1" = 1200'



<p>TOWN OF LEWISBORO TOWN PLANNING BOARD Frederick P. Clark Associates, Planning Consultants</p>	<p>SOUTH SALEM SPECIAL CHARACTER AREA</p>	<p>NORTH FIGURE 14</p>
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- d. All new construction, residential and commercial, proposed within the area boundaries should be subject to architectural review. With few exceptions, the existing structures on Spring Street, Main Street and Bouton Road respect the historic setting through design or buffering. If those few exceptions are joined by new discordant buildings, the visual image of a drive through South Salem could quickly change and lose its noteworthiness.
5. West Lane/Elmwood Road. Unlike the previously discussed areas of special character, the key characteristics of the West Lane/Elmwood Road area do not readily stand out. But a drive along these roads produces a feeling of uniformity. That feeling is attributable to the stone walls and large trees lining the roadside and the spatial relationship of several large homes to open fields. Although much of the existing architecture is diverse, the landscape elements are shared throughout the area.

This area, more than any of the others, must be guided to realize its potential as an area of special character. Through the use and maintenance of the landscape elements, much of Elmwood Road can achieve the present visual quality of West Lane.

The following policies are established for the West Lane/Elmwood Road area as shown on Figure 15:

- a. The stone walls and large trees which line much of the length of West Lane and Elmwood Road are to be maintained as key landscape architectural features. Restoration of the many sections now overgrown or in disrepair should be encouraged.
- b. The rural density flavor of this area should be continued. Emphasis should be placed on the separation of buildings, setback from road and protection of open field areas.
- c. There is a diversity of building style in the area which reduces the need for detailed architectural review. However, review of the proposed bulk and exterior finish of new structures should be conducted to ensure compatibility with the immediate area.

#### D. Landmarks

Lewisboro has many notable individual buildings, many of historic interest, which are not located within one of the special character areas outlined above. Their individual and collective importance to the community is fully recognized by the community groups have been engaged in efforts to





The isolated location and relatively small size of landmark structures prevents individual discussion here. Nevertheless, policies can be established to provide guidance for Town agencies whenever such structures become involved in a development application or public issue.

The guidelines for the protection of landmark structures are:

1. Preservation. The destruction or inappropriate alteration of structures recognized as community landmarks by local historical groups or the Town historian should be discouraged and, when possible, prevented.
2. Zoning. Appropriate zoning regulations, consistent with the goals and policies of this Plan, should be developed and implemented for the purpose of promoting the preservation of landmark structures.
3. Development. When landmark structures are included as part of a subdivision or site development application, efforts should be made to not only retain the structure but to protect an appropriate setting.

APPENDIX B

ACARC ENABLING LEGISLATION

LOCAL LAW #3-1987



TOWN OF LEWISBORO

WESTCHESTER COUNTY, NEW YORK

LOCAL LAWS AND ORDINANCES

Kathleen G. Cory, Town Clerk  
Main Street, South Salem, NY

LOCAL LAW NO. 3-1987

ESTABLISHING AN ARCHITECTURE AND COMMUNITY  
APPEARANCE REVIEW COUNCIL

1. Legislative Intent

The Lewisboro Town Board hereby finds that excessive uniformity or dissimilarity or inappropriateness or poor quality of design in the exterior appearance of buildings, other structures or land developments adversely affects the desirability of the immediate area and neighboring areas and by so doing impairs the benefits of occupancy of existing property in such areas, impairs the stability and value of both improved and unimproved real property in such areas, adversely affects the most appropriate development of such areas, produces degeneration of property in such areas with attendant deterioration of conditions affecting the health, safety, morals and general welfare of the inhabitants thereof, and creates an improper relationship between the taxable value of real property in such areas and the cost of municipal services provided therefore. It is the purpose of this local law to prevent these and other harmful effects of such exterior appearance of buildings, other structures and land developments, and thus to promote and protect the health, safety, morals and general welfare of the community.

The Town Board recognizes and respects the right of property owners to develop their property according to their own tastes consistent with the intent of this ordinance.

2. Council Established; Organization

There is hereby created an Architecture and Community Appearance Review Council (ACARC) which shall consist of seven (7) members who shall be residents of the Town of Lewisboro, and who shall serve without compensation, and who shall be specifically qualified by reason of training or experience in a relevant business or profession or by reason of civic interest and sound judgment to assess the effects of a proposed structure or the alteration or demolition of a structure upon the property values and development of surrounding areas. Two (2) members of the Council shall be licensed architects. The members of the Council shall be appointed by and the chairman thereof shall be designated by the Lewisboro Town Board. Members of the ACARC shall serve at the pleasure of the Town Board. Each member shall be appointed for a term of three (3) years, except that the initial appointment of two (2) members shall be for one (1) year, of two (2) members for two (2) years, and of three (3) for three (3) years. Vacancies shall be filled by the Town Board for the unexpired term of any member whose seat has become vacant.

### 3. Jurisdiction

- A. The following matters shall be referred to the ACARC for its approval, conditional approval or disapproval.

Within a Hamlet Preservation District or Special Character Overlay District:

Any application for a building permit for construction, reconstruction, exterior alteration or demolition of buildings or other structures including signs, fences or walls;

Any application for a variance from the Zoning Ordinance of the Town of Lewisboro or for a special use permit.

8. The Planning Board shall refer to the ACARC for its review and recommendation for Planning Board approval, conditional approval or disapproval plans for construction or exterior alteration of any commercial or business building or structure, or multifamily residence contained in any application for site plan approval pursuant to Section 350 of the Zoning Ordinance of the Town of Lewisboro.

- C. The Planning Board, prior to granting preliminary approval, may refer any item to the ACARC for its review and recommendation.

- D. The Zoning Board of Appeals may refer to the ACARC for its review and recommendation any application for a variance from the Zoning Ordinance of the Town of Lewisboro or application for a Special Use Permit.

- E. The Town Board shall refer to the ACARC for its review and recommendation plans for construction, exterior alteration, or addition to Town of Lewisboro municipal or recreational structures or facilities.

### 4. Meetings and Procedures

Regularly scheduled meetings of the ACARC shall be held monthly and at such other times as the chairman shall designate. A majority of the Council (four members) shall constitute a quorum for the transaction of business. The ACARC shall keep minutes of all its proceedings. Any decision or recommendation made by the ACARC must have the approval of a majority (four) of the members of the Council and shall be officially filed along with pertinent data upon which such decision or recommendation was based with the referring board or department within forty-five (45) days of the Council's receipt of the referral. In decisions of conditional approval or in recommendations where changes or modifications to any submission are required or recommended, the Council's report shall set forth the specific changes required or recommended for same. After having referred an

ACARC, the referring board or department shall take no final action prior to receiving and considering the recommendations of the Council. Failure of the ACARC to report within forty-five (45) days shall be deemed to be a recommendation for approval of the application. The referring board or department shall provide necessary plans and pertinent data to the ACARC for its use. The ACARC may request any applicant to appear.

#### 5. Guidelines for Recommendations

In making its decisions and recommendations, the ACARC shall consider the following:

No building or structure or land development shall be so detrimental to the desirability, property values, or development of the surrounding areas as to provoke one or more of the harmful effects set forth in paragraph 1 by reason of:

A. The repeated and adjacent use of identical or near identical facades or structures arranged without respect to natural features of terrain or other existing structures.

B. The inappropriateness of a structure or land development in relation to any other structure or land development existing or for which a permit has been issued or to any other structure or land development included in the same application with respect to one or more of the following features:

Gross differences in cubical content floor area, height of building or height of roof and;

Other significant design features such as material or quality of architectural design, roof structures, structures, chimneys, exposed mechanical equipment and service, service and storage enclosures, signs, landscaping, retaining walls, parking areas, service and loading docks, dividing walls, fences, lighting posts and standards.

A finding of inappropriateness shall state not only that such inappropriateness exists but further that it is of such a nature as to be expected to provoke beyond reasonable doubt one or more of the harmful effects set forth in paragraph 1.

#### 6. Effect of Action by ACARC

The Building Inspector shall refuse any permit application disapproved by the ACARC under provisions of section 3A above. The Building Inspector may approve any application conditionally approved by the Council as soon as the condition(s) specified in such conditional approval has (have) been fulfilled. If the Council shall have approved any building permit application,

then the Building Inspector may forthwith issue the permit as applied for. The Building Inspector may issue a permit as applied for in the case of any building permit application referred to the Council and upon which the Council has failed to act within forty-five (45) days of such referral.

7. Appeals from decisions of ACARC

Any person aggrieved by the action of the ACARC in disapproving a building permit application and or of the Building Inspector in denying such permit because of such disapproval, may take an appeal to the duly constituted Board of Appeals in the same manner as is provided for zoning appeals. Such Board of Appeals, after proceeding in the same manner as is provided for zoning appeals and with the same power and authority therein vested in passing upon appeals before it under the provisions of law, and in the exercise thereof, may reverse or affirm or modify and affirm the action of the ACARC and/or of the Building Inspector.

## APPENDIX C

### General Guidelines

For the Review of Lewisboro Architecture, Landscape  
and Community Appearance Issues

ACARC

October, 1990

The general guidelines, for ACARC use in review questions outside of the SC Districts are based upon lessons learned in the analysis of the SC Districts, which are in fact, common to the entire town.

ARCHITECTURE AND COMMUNITY APPEARANCE AND REVIEW COUNCIL

TOWN OF LEWISBORO

GENERAL GUIDELINES  
FOR THE REVIEW OF LEWISBORO ARCHITECTURE, LANDSCAPE  
AND  
COMMUNITY APPEARANCE ISSUES

INTRODUCTION

The Special Character District Analysis and Guidelines seek to identify the elements specific to those areas that make that District special. However, many of those elements occur, not just in the SC Districts, but throughout the Town. Thus, the Special Character District Guidelines can be applied in a broader sense to the Town in general and form the basis for General Guidelines.

ACARC's review responsibilities outside of the Special Character Districts is limited to commercial and multi-family developments, construction or renovation of Town facilities, or other referrals by the Town Board or Planning Board. It is the intent of the General Guidelines to provide a framework for ACARC review of these issues.

In the following passages, the same elements observed in the SC Districts, including topography and landscape, roadways, views, special places, houses, other uses and traffic are considered on a town-wide basis. They form the basis for the General Guidelines.

1. TOPOGRAPHY/LANDSCAPE

Lewisboro is not about buildings, it is about landscape. The natural green, rural environment is central to Lewisboro's character.

Natural, rolling topography

The existing natural, rolling topography should be preserved whenever possible. Ridges, hills and wetlands should not be built on. New construction should not alter the topography but should nestle in and compliment it.

Tree canopy/wooded watersheds

The existing tree canopy should be protected and preserved. The trees are essential to the integration of buildings and landscape enjoyed in the town.

## 2. ROADWAYS/PUBLIC WAYS

Roads form the public view corridors through which people observe the Town and its environs.

Roads follow natural contours, grades- Most of the Town's roads follow the original routes on which they were laid out, and tend to rise, fall and curve with the natural contours of the land. This integration of roadway and landscape is a positive characteristic that should be preserved.

Roadway definition - Elements such as formal or informal tree lines, stone walls, fences and grade changes add definition to roadway edges, and heighten the integration of roadway and landscape. Such features should be preserved.

Curbs - Most Town roads have "natural" shoulders, without curbs, which is consistent with their rural character. Curbs suggest urban and suburban settings and should not be encouraged in Lewisboro. Curbs are discouraged, but where new development roadways require curbs, it is suggested that the curbs be held back from intersections with existing rural roads.

Sidewalks - Sidewalks, like curbs, are rare in Lewisboro, which is appropriate in a rural setting. Where sidewalks are felt to be needed, it is suggested that they be set away from the roadway edge, and separated from it by lawn or landscape.

Roadside Railings - There are a variety of types of roadside railings used in the Town, as well as in the Westchester area. Some types of railings are more appropriate to their rural setting than others. Where practical, assemblies of wood or concrete posts and wood or cable rails are to be encouraged. The continuous metal bonded rail, more commonly seen in highway construction today, is to be discouraged.

Street Signs - Street signs of painted wood, such as those that mark many Town of Lewisboro facilities and some neighborhoods aptly reflect the rural character of the town. These should be preserved and maintained. The more typical Department of Transportation green metal street signs should be discouraged whenever practical.

Unpaved Roads - Existing unpaved, earthen roads exemplify the Town's original rural character. Where practical, they should be preserved.



### 3. VIEWS

From a community standpoint, it is the views that occur in relationship to public corridors, i.e. roadways, that are important.

Views parallel to roadways - Views down roadways, parallel to the road, often framed by tree lines and focused on some distant landmark are of special interest and should be preserved.

Views perpendicular to roadways - Views from roadways to natural features such as meadows, hills or reservoirs, as well as to man made landmarks are also of interest and should be preserved.

### 4. SPECIAL PLACES

Special places, such as those cited in the SC Districts, are points of particular interest within the otherwise generally residential/rural fabric of Lewisboro. These places have one or both of the following characteristics.

Historic/special interest - Historic places or landmarks, whether homes, churches, or cemeteries, whether open to public visits or just to public view are of general interest and often of aesthetic interest as well.

Public uses/activities - Schools, libraries, municipal buildings, facilities and parks can also be special places. Such places should be designated and special care given to their appearance.

### 5. HOUSES

The 1985 Master Plan defines Lewisboro as a predominantly residential community. Appropriately, it is houses and their related outbuildings that define the character and in particular the scale of the Town's built environment. It is the scale of the house that allows for the integration of the built and natural environments that we see in the Town. It is this integration that should be protected and promoted. Criteria regarding houses follow.

Scale - Scale is a function of massing, height and size. Houses of two to two and one-half stories are encouraged. Pitched roofs are encouraged. New houses built in close proximity to existing structures should relate closely to the scale of the existing structure.

Siting - Houses set in close proximity to other houses will respect existing patterns of setback from street lines and property lines. Isolated houses will seek integration with the existing landscape.

Style - With the wide variety of historical and contemporary architectural styles in Lewisboro, style is a less important issue. It is critical only where new construction is in close proximity to existing buildings of notable historical style (often in SC Districts), on sites prominent from public ways.

## 6. OTHER USES

Uses other than the residential or special uses cited above can have both positive and negative impacts on the Town. The negative impacts require mitigation measures. As seen in the SC Districts, successful integration of non-residential uses is achieved in two ways. The first way is to keep the other uses in buildings of residential scale. The second way is to screen the other uses with landscape buffers. Guidelines for specific uses follow.

Local office/retail - Small local service uses, such as individual shops or professional offices benefit the Town's hamlets and are particularly successful when located in existing, renovated houses. The use of existing houses and other structures is strongly recommended in lieu of new construction.

Larger scale uses - Uses such as multi-family housing, larger scale office, commercial or industrial concerns, as well as recreational or maintenance facilities are more difficult to fit into Lewisboro's residential and natural landscape context. The following measures are suggested to ease this fit, which again speak to the goals of landscape integration and residential scale.

Low scale/low profile - Visually prominent development is discouraged. New construction should "tuck" into the landscape via low scale, grading and landscape treatment, per the goals of "campus" style development of the 1985 Master Plan.

Landscape buffering/integration - Effective landscape buffers, at best of natural tree canopy, in all cases within the property of the use to be screened are strongly encouraged, especially where different uses are adjacent.

Reduction of scale - The breakdown of larger buildings into massings of residential scale and form is strongly encouraged. Pitched roofs are encouraged.

#### 7. TRAFFIC

Traffic - Vehicular traffic of all kinds is increasing in the Town. While more an engineering issue than an aesthetic issue, accommodations for traffic have significant impacts on the visual environment. Without compromising safety, the benefits of smooth flowing, higher speed traffic should always be weighed against the more timeless pleasures of rural routes, tree lines, stone walls and other elements that make Lewisboro an attractive environment.

#### CONCLUSION

It is believed that the application of these General Town Guidelines to the review process will go far to both protect and enhance the special character that exists throughout Lewisboro.

APPENDIX D

ACARC PROCEDURES

October, 1990

# ARCHITECTURE AND COMMUNITY APPEARANCE REVIEW COUNCIL

## PROCEDURES

### 1. PURPOSE OF ACARC

ACARC was established in 1987 to assist the Town in making the aesthetic judgements that affect the appearance and character of Lewisboro's built and natural environments. ACARC reviews projects referred to it by the Planning Board, the Town Board, or the Building Department for appropriateness with respect to review criteria developed by ACARC.

Town of Lewisboro Local Law No. 3-87, establishing ACARC, is referenced.

#### ACARC Council Members

The council consists of seven (7) Lewisboro residents, including two (2) registered architects.

Council members are appointed by the Town Board for terms of three (3) years.

#### ACARC Chairman

- The chairman drafts meeting agenda.
- The chairman conducts meetings.
- The chairman assigns tasks to council members.
- The chairman signs ACARC communications and decisions.
- The chairman appoints acting chairperson in the event of his absence.

#### ACARC Secretary (Assigned by the Town)

- The secretary writes ACARC monthly meeting minutes, and sees to their typing, distribution and if required, revision.
- The secretary maintains a file of ACARC communications, actions, minutes, and correspondence for ACARC use.

### ACARC Meetings

- Regular ACARC meetings are held monthly. Meeting time and place is listed in local newspaper and posted at the Town House and Onatru Farm.
- Meetings are open to the public per the requirements of the Open Meetings Law.
- Minutes are recorded and adopted at the following meeting.
- Submissions referred to ACARC are received and examined for completeness; field trips relating to the submissions are scheduled, if necessary. Submissions discussions are scheduled.
- Submissions are reviewed, comments are compiled, approval or disapproval is determined by vote. A resolution to record specific ACARC action and comments is written by the chairman or his designee. The formal resolution is processed and forwarded to the appropriate town department within forty-five (45) days of ACARC's receipt of the submission.

### Quorum/Action

- Four (4) attending council members constitute a quorum.
- Actions are taken by simple majority vote.

## 2. ACARC INTERACTION WITH OTHER TOWN DEPARTMENTS

### Town Supervisor

- Receives copies of agenda, actions, decisions and minutes.

### Town Board

- Appoints ACARC council members.
- Town Board may refer plans for construction, exterior alteration or additions to town municipal/recreational structures or facilities.

### Planning Board

- Planning Board, refers multi-family, commercial, and Special Character projects to ACARC.

## 3. REVIEW PROCEDURES

### Planning Board Referrals

ACARC is notified of projects that will require its review.

- ACARC Chairman receives copy of sketch plan submission and assigns council member to attend sketch plan presentation.
- Council member briefs ACARC on sketch plan presentation at next ACARC monthly meeting. If appropriate, ACARC studies plans, visits site in advance of formal Planning Board referral.
- ACARC receives formal Planning Board referral at monthly meeting, reviews submission. The Chairman or his designee writes a response outlining ACARC concerns on submission to be forwarded to Planning Board within fifteen (15) days.

### Building Department Referrals

- ACARC Chairman receives referral from Building Department, checks submission for completeness, and schedules discussion at regular monthly meeting allowing time for site visits, if required.
- ACARC reviews submission, the Chairman or his designee writes a response outlining ACARC action on submission to be forwarded to the Building Department within fifteen (15) days.

### Town Board Referrals

- ACARC Chairman receives referral from Town Board, checks submission for completeness and schedules discussion at regular monthly meeting allowing time for site visits, if required.
- ACARC reviews submission, the Chairman or his designee writes a response outlining ACARC action on submission to be forwarded to the Town Board within fifteen (15) days.

#### 4. SUBMISSION REQUIREMENTS

##### General

Existing conditions including buildings, roads, grading, major trees, stone walls and other landscape elements are to be clearly documented and identified.

Drawings are to be to scale, titled and dated. Nine (9) sets of drawings should be submitted to ACARC.

##### Required Documentation

Documentation requirements will vary with the nature of the project. The full range of submission materials may include:

- Site Plan/Location Plan
- Landscape/Planting Plan with species and sizes of proposed plant materials
- Floor Plans, Elevations
- Perspective View
- Profile Section showing nearby buildings
- Materials, samples, colors
- Scale Model